Robert Bird Group

Member of the Surbana Jurong Group

Kennet Shopping Centre, Newbury

Demolition Desktop Appraisal

Prepared For:
Lochailort Newbury Ltd

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4508

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REVISION AUTHOR:

.....
C. Ennis
Signing for and on behalf of
Robert Bird & Partners Ltd
Date: 14/10/2019

REVISION CHECKER:

.....
[D. Abrahams]
Signing for and on behalf of
Robert Bird & Partners Ltd
Date: 14/10/2019

REVIEWER/APPROVER:

.....
D. Abrahams
Signing for and on behalf of
Robert Bird & Partners Ltd
Date: 14/10/2019

Document2

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1.0 Introduction

Lochailort Newbury ltd (LNL) are looking to acquire a 6-acre site in the centre of Newbury town centre for re-development. The site current site comprises of the 'Kennet Centre' a retail and leisure development. The Kennet centre development can be broadly split into 3 main areas; Retail, a multi-storey car park and Vue multi-screen cinema. The site is approximately trapezoidal in shape and is bound on three sides by roads; Cheap St, Bartholomew St and Market St. The site has a number of interfaces with adjoining properties, to the North is a Victorian retail arcade and within the wider trapezoidal footprint is indented by adjoining retail units and pubs.

Figure 1: Kennet Centre Site

LNL are looking to demolish the bulk of the site comprising of retail area and redevelop into a mix of low-medium rise residential units whilst retaining the Cinema and multi-storey car park (highlighted in green in figure 1).

LNL have commissioned Robert Bird Group (RBG) to conduct a pre-demolition study to determine the impacts of demolishing most of the site whilst retaining the cinema and car park.

2.0 Report Scope

The scope of this report is to conduct a high-level desktop assessment of the potential partial demolition of the Kennet Centre. The desktop review will focus on the historic information made available from LNL, information gathered during an earlier site visit and an online review of historical information.

RBG understand that the 'Kennet Centre' has been developed over multiple phases since the 1970's. Understanding the history of the development will greatly assist in the demolition feasibility assessment as it will identify which parts are structurally independent. As such the first section of the report will discuss the Kennet's centres development.

Section 4 will identify the assumed structural systems used within the various development phases, focusing on the form of construction and likely stability systems.

Section 5 will then interrogate the key interfaces between the different phases as well as the interface to adjoining structures. Throughout the report 'Figure 2: Key Interfaces' below will be cross-referenced to identify the specific area's being discussed. Interfaces A1-A8 focus on the early phases of development with adjacent or adjoining structures. Interfaces B1-B2 relate to the retained multi-storey car park and interfaces C1-C2 will focus on the interfaces with the cinema.

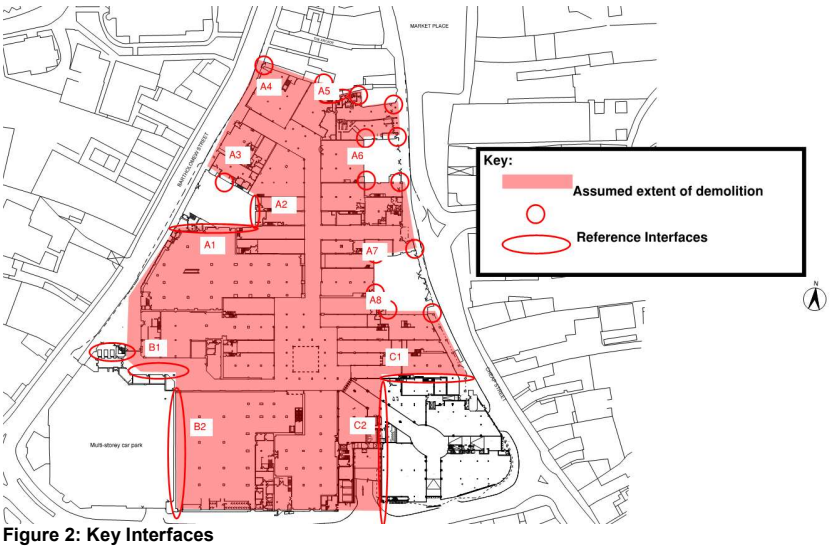


Figure 2: Key Interfaces

Section 6 will assess the feasibility of the proposed partial demolition with a focus on structure. The desktop study is inherently high-level and should be used for discussion only and not to be dependent for pricing. Section 6 will identify the feasibility, envisaged impacts on demolition and critical areas where further investigation should be conducted.

3.0 Site History

The 'Kennet Centre' in its current form has been developed over multiple phases since the 1970's. This has been broadly split into 2 parts the earlier phases assumed to be developed in three stages from 1973 to 1983, these phases cover the retail (phase 1 &2) and Multi-storey car park and remaining retail (phase 3). Phase 4 covers 2009 extension including a multi-screen cinema.

This section should be read in conjunction with the timeline sketches provided within appendix A.

3.1. Early development phases (Phases 1,2&3)

Development of the 'Kennet Centre' started in 1973 with Phase 1, and shortly followed by Phase 2 assumed completed in 1976, Phase 3 was developed about 10 years later in 1983.

Phase 1 extended from market street to the south of the site where the vehicle ramp servicing the retail back of house areas begins, as part of this phase the building formerly the Sainsbury's was constructed to the left-hand side of the ramp. Retail units 1-23 were constructed as part of phase 1. Shortly after Phase 2 incorporating retail units 24-44 was constructed to the North of Phase 2 with a movement joint extending east west across the site. Phase 2 extended north to the Victorian retail arcade on the northern boundary of the site.

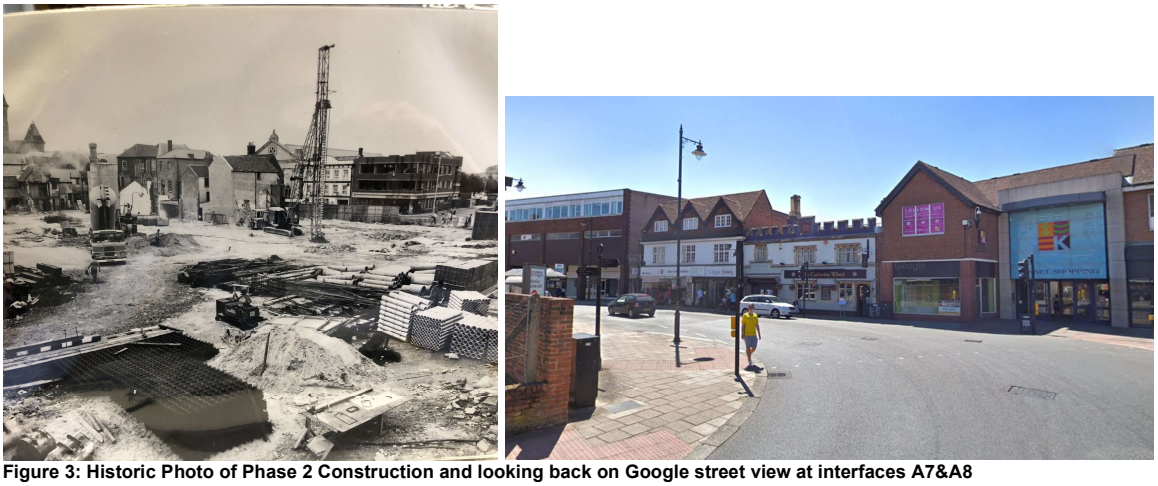


Figure 3: Historic Photo of Phase 2 Construction and looking back on Google street view at interfaces A7&A8

Figure 3 of Phase 2 of the development is looking North-North-East the building on top right of the picture is at the junction of Bear Lane and Cheap St. In this photo we can see both the A6 and A5 interfaces illustrated on Figure 2. Clearly from this photo the adjoining properties at interfaced A5 and A6 are structurally independent of the historic Kennet centre development. Additionally, from interrogating interface A8 there is physical separation between the existing structures.

Figure 4: Bartholomew St and North West corner of Site Prior to Early Phase development

Figure 4 is looking north along Bartholomew St in advance of the early phases of development looking at where currently stands the 'The Newbury' public house interface A1 on Fig 2. Figure 5 below of the Phase 3 looking west from the site of the Phase 3 construction looking at Bartholomew St with 'The Newbury' on the left. Both images indicate that 'The Newbury' was a free-standing structure in advance of the Kennet centre development.

Figure 5: Bus Station prior to Phase 3 development

Phase 3 in 1983, developed the western portion of the site adjacent to Bartholomew St, this portion of the site was utilised as a bus garage up till the Phase 3 development as indicated in figure 5.

Figure 6: Phase 3 Construction looking at Bartholomew St

Figure 7: Phase 3 from Bartholomew St 1990's

Figures 6 and 7 show the Phase 3 portion of the Kennet centre under construction and in operation during the 1990's respectively. As part of the Phase 3 development the multi storey car park (to be retained) was constructed, a portion of the car parking provision provided during this phase was constructed on the roof of a retail unit indicated in Black on figure 1.

3.2. Phase 4: Cinema

The latest extension to the Kennet centre (2009) consisted of a multi-screen cinema and 5 restaurant units over 3 storeys. The 6,000m^2 extension is positioned in the South East corner of the development on a former car park. The Phase 4 development required the relocation of a substation from under the ramp access to the left of the ramp adjacent to the ramp. Figures 8 and 9 below show the pre-phase 4 development and then the new structure intertwined between the ramp and former Sainsbury's unit where the substation now is located.

Figure 8: Phase 1 in advance of interfacing Phase 4 construction

Figure 9: Phase 4 interface with Phase 1; view of ramp and former Sainsbury

4.0 Assumed Structural Systems

RBG's assessment is based upon high level observations and assumptions regarding likely forms of construction to be adopted. A more in-depth intrusive survey would be required to determine the true form of the various structural systems which is beyond the scope of this report.

4.1. Early development phases (Phases 1,2&3)

Phases 1 & 2 of the development appear to have been constructed in very similar forms of construction. Figure 10 is a picture of the Phase 2 construction shows a piling rig in use, this is significant in the fact that the early phases are only 2 storeys and could potentially have been founded on shallow foundations (e.g. PAD footings). Piled foundations mean that the weight from the structure is founded at depth so will have less impact on the adjoining structures in both construction and now in demolition. The adjoining properties at interfaces A1-A8 due to their age

will be founded on shallow foundations, the difference in foundation systems is a positive indication that they are not shared.

Figure 10: Piling Rig construction Early Phases of Kennet Centre

The initial site walk around showed that the initial phases of retail development are of RC frame construction typically on a 6m grid. Figure 11 is a picture taken within Phase 2 in unit which sits in the North-West corner of the site with Bartholomew St shows a RC frame construction with a beam RC grillage. The fire stair access shown at the back of the unit is possibly to be also of RC construction and could be providing lateral stability to this phase of development.

Figure 11: Phase 2 Retail Unit RC frame

The bulk of the early phases were constructed as single storey RC frame with load bearing masonry storage/BOH area's at Level 1 podium level. As can be seen in figure 12 a movement joint splits Phases 1 and 2. Phases 1 and 2 are sheltered from wind by adjacent properties, the low-rise nature and heavy structure (RC) means these phases are not particularly susceptible to lateral instability.

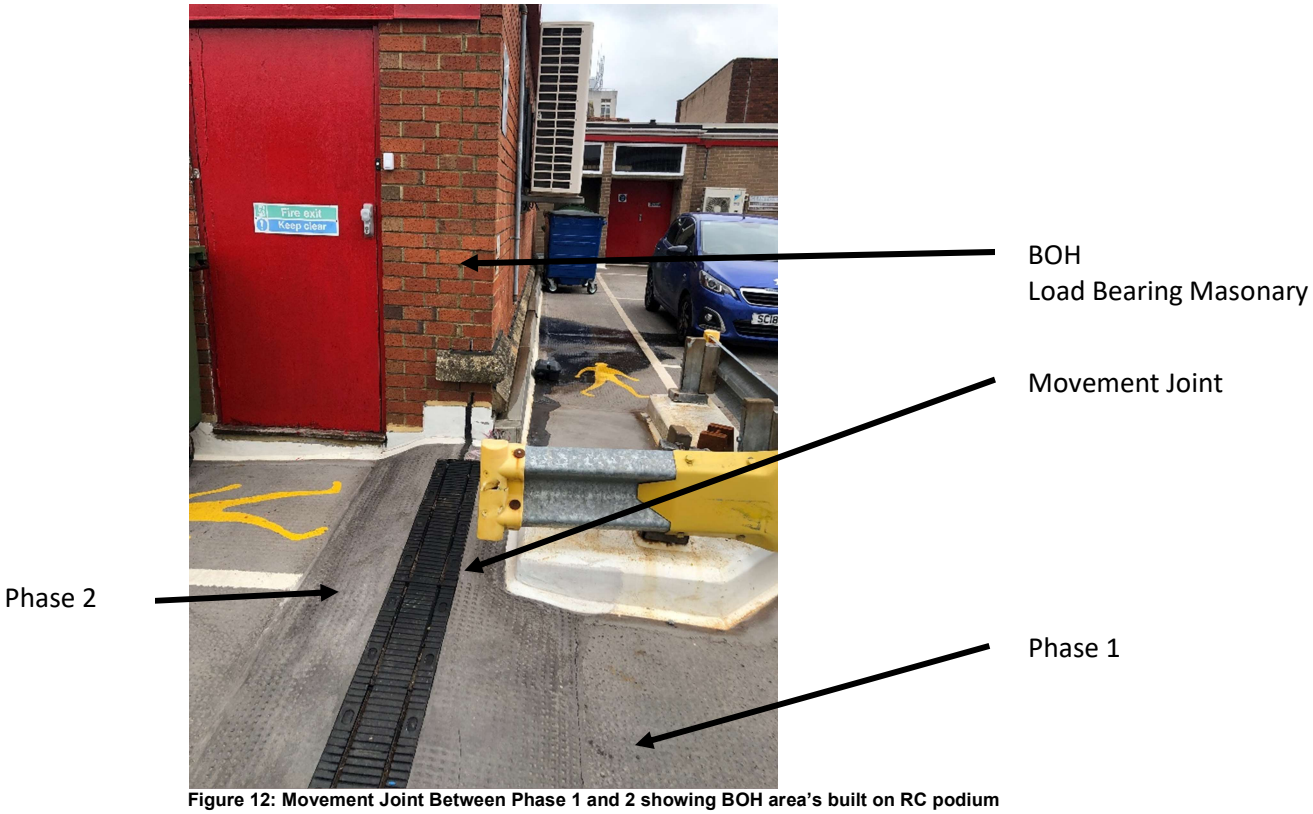


Figure 12: Movement Joint Between Phase 1 and 2 showing BOH area's built on RC podium

The Phase 3 development is split between the additional retail units and the multi-storey car park. There is little drawing information relating to retail portion of Phase 3. However, figure 13 which is a ground floor drainage drawing for phase 2 shows the proposed Phase 3 development (cyan). From this drawing we can see that the grid will follow closely that of Phase 2 an would lead us to assume that the Phase 3 retail was constructed in a similar manner to both phase 1 and 2.

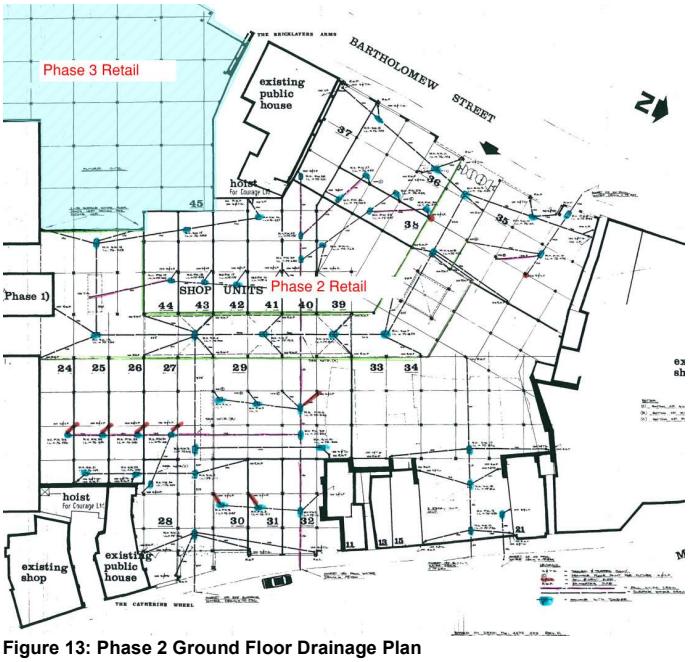


Figure 13: Phase 2 Ground Floor Drainage Plan

Historic drawing information of the multi-storey car park and photo's taken during an earlier site visit indicate that the 3-storey portion of the car park is structurally independent of the 2nd floor roof deck at least in the North -south direction where a movement joint (M.J.) exists as indicated in figure 14 below.

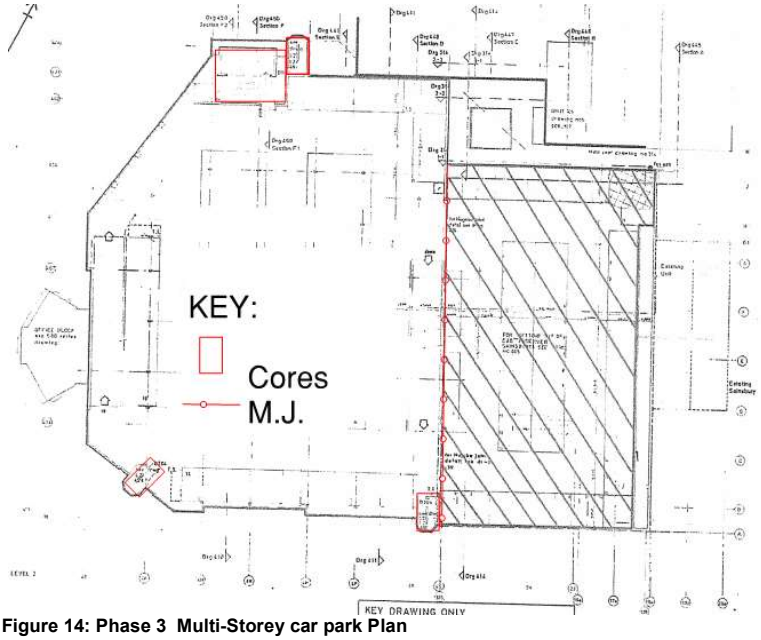


Figure 14: Phase 3 Multi-Storey car park Plan

The 3-storey car park looks to utilise the cores highlighted in red on figure 14 for lateral stability. They are constructed as RC band beams and coffered slabs, presumed to be founded on pile foundations as with the earlier phases.

4.2. Phase 4: Cinema

The 6,000m^2 extension is of steel framed construction, on piled foundations as indicated on the contractors (Sisk) website. We can reasonably assume the lateral stability Is provided in the form of braced bays and/or braced cores, all though at this time these have not been specifically identified.

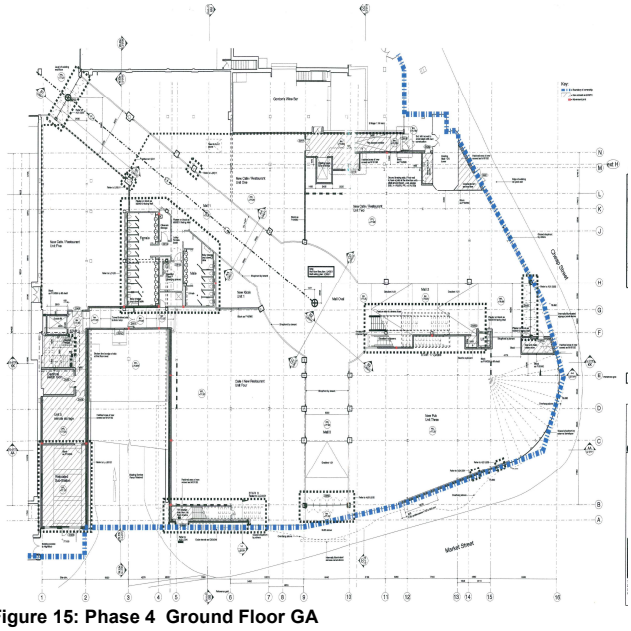


Figure 15: Phase 4 Ground Floor GA

Majority of the phase 4 site was constructed on a former car park positioned to the east of the Phase 1 vehicle ramp (gridline 5). A portion of the phase 1 retail was redeveloped but appears to have retained the ground floor structure including the ramp. The substation formerly beneath the ramp was relocated to the left side of the ramp on the Market St elevation, the remaining infill area between the ramp and former Sainsbury's unit is a 2-storey steel frame structure. It can be reasonably assumed that due to the ramp intersecting the phase 4 development the structure either side of gridline 5 are independent in terms of structural stability.

5.0 Critical Interfaces

Critical interfaces discussed within this section will focus on those identified in figure 2. These have been broadly split into three area's due to their significance in relation to the proposed demolition.

- A: Early phases 1-3 and how they interact with the structures that adjoin them or sit close to the site boundary
- B: Interface of retained multi-storey car park and the phase 3 retail structure to the north and East
- C: Phase 4 Cinema's interfaces with Phase 1

It is beyond the scope of this document to interrogate interfaces with third party assets such as highways or sewers. Service diversions especially those from the substation adjacent to the ramp are likely to be required but do not fall within the scope of this document.

5.1. A: Early Phases to adjoining properties

During the desktop study there was little detailed structural information available relating to the Phase 1 and 2 developments. The A1-A8 interfaces are extensive and are highly variable as they adjoin several different properties presumed to have been constructed over a long period of time dating back many years 100+.

However, a search of historic photo's has proved useful in identifying that many of the structures at the A1-A8 interfaces appear to have been free standing independent structures in advance of the Kennet Centre development. These are clearly visible within Figures 3,4,5&6 and there is further evidence of this gathered in appendix C.

We can also assume that based on a piling rig visible on figures 3 & 10 the Kennet Centre is founded on piled foundations at depth. Based on this assumption it would be reasonable to assume that the foundations at these interfaces are structurally independent. It is also reasonable to assume that the RC frame structure differs greatly from the forms of structure of adjoining properties likely to be traditional load bearing masonry construction. As a result, it is very unlikely that the Kennet centre is dependent on adjoining structures for structural support.

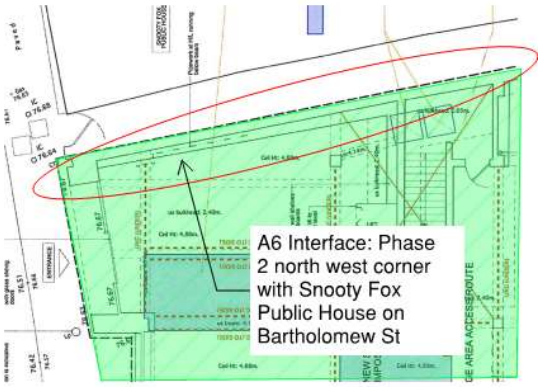


Figure 16: Phase 2 North West Corner

A renovation drawing provided (figure 16) within the LNL information shows the unit within the furthest North West corner (A6) adjacent to 'The globe Inn' public house. This drawing clearly shows as assumed the phase 2 development is structurally independent of the adjoining structure at this interface.

5.2. B: Phase 3 Multi Storey Car Park to Retail

This interface is in many ways the simplest to interpret as we have some record drawing information of the structure in this area. The photograph in figure 17, looking at the movement joint dividing the multi-storey car park with the roof car parking above the supermarket unit presumed to be constructed as an extension to the supermarket visible in figure 8. The detail of which (figure 18) was found within the historic drawing information and correlates to interface B2.

This movement joint corresponds conveniently with the proposed demolition line along that elevation.

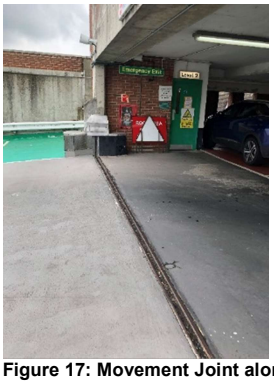


Figure 17: Movement Joint along interface B2

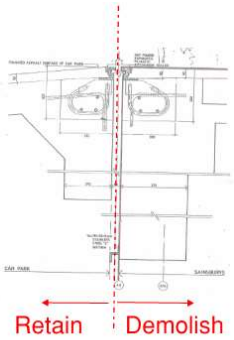


Figure 18: Detail of Movement Joint along interface B2

Unfortunately, the northern interface B1 is not as clearly defined on the record drawing information available nor was this an area inspected during the earlier site visit. We cannot confirm at this stage whether there is a movement joint along this interface to determine whether this interface is structurally independent.

5.3. C: Phase 4 Cinema to Phase 1

The interfaces of phase 4 to Phase 1 are in many ways the most complex. Fortunately, the drawing information available for the phase 4 development is the clearest to understand.

The northern boundary C1 is clearly shows the structures are independent as can be seen in photo of figure 19 there is clear structural separation.

Figure 19: Interface C1

The interface with phase 1 and phase 4 as previously described in section 4.2 as they are intertwined. Fortunately, as indicated in figure 20 the green outline representing the phase 4 first floor structural outline shows a clear split from the cinema to the 'infill area' constructed between the ramp and the former Sainsbury's supermarket unit.

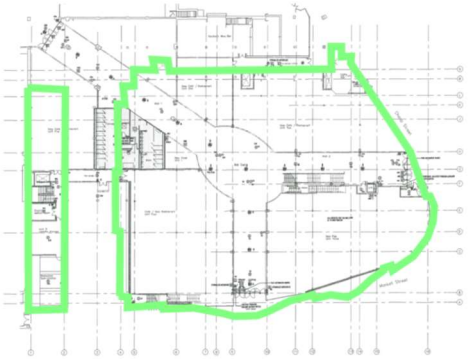


Figure 20: Phase 4 First Floor Outline with Ground Floor

The superstructure of phase 4 is divided by the access ramp which LNL intend to demolish. Looking at this interface C2 at more depth from the record drawings it is clear that the columns supporting the new phase 4 structure within unit V5 are completely independent to the phase 1 ramp.

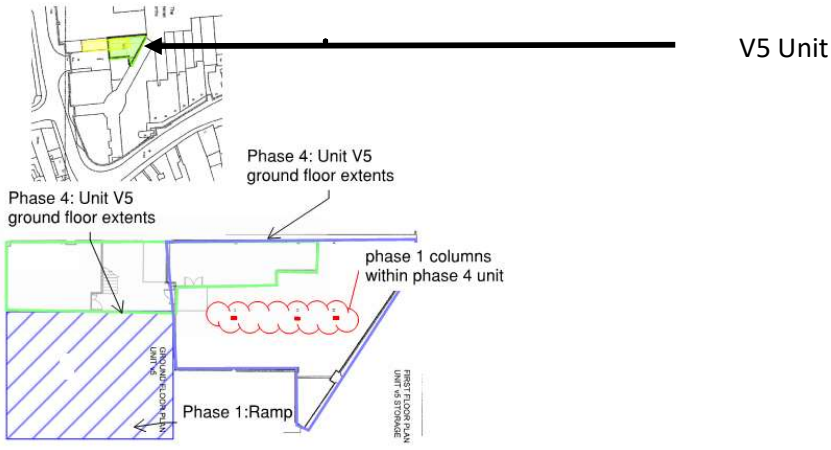


Figure 21: Unit V5 and Phase 1 Ramp

As can be seen in figure 21 the phase 1 and 4 structures are independent. As the ramp splits the phase 4 construction along the proposed demolition line this would infer that the phase 4 structure west of gridline 5 could be demolished without affecting the global stability of the Cinema.

Interrogating the drawing information available for the phase 4 team there appear to be settlement joints and columns of the 2 phases positioned along a common line. This infers there is structural

separation. A high level review of the Phase for GA drawing would indicate a suitable demolition line along the C2 interfaces, highlighted by the green line on Figure 22.

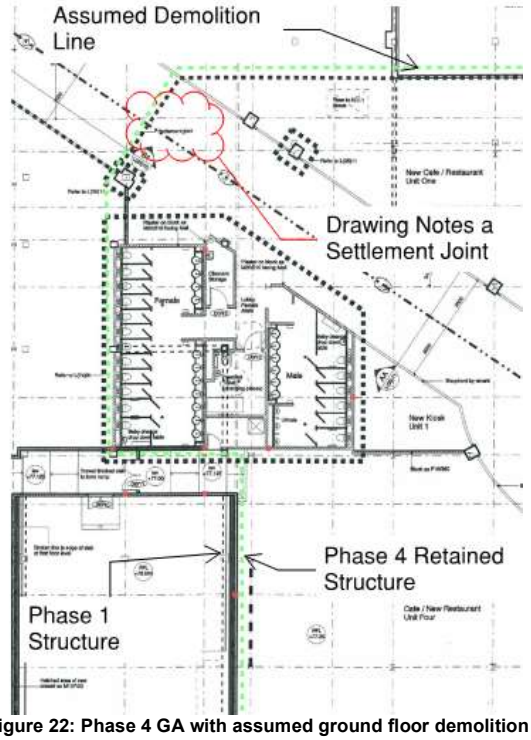


Figure 22: Phase 4 GA with assumed ground floor demolition line

6.0 Demolition Feasibility Assessment

As has been discussed previously the Kennet centre has been developed over multiple phases over decades. The historical structural drawing information available at this time is very limited and so it has been important to understand the history of the phased developments in order to inform our understanding of the interfaces and as such understand the impact demolishing large sections of the Kennet Centre.

Most of the demolition will occur in the northern portion of the site and will impact the A1-A8 interfaces as illustrated on figure 2. From the information available we can reasonably assume that the Kennet Centre Phases 1-3 are structurally independent of the party wall structures to the North of site and those along Bartholomew St and Cheap St. Whilst structurally independent these interfaces in many instances abut one another and the demolition strategy is to be developed cognisant of this especially in relation to noise and vibration effects of adjacent properties during demo. Whilst we are confident that the foundations are independent at these interfaces and likely to be only local to the column positions, care should be taken when grubbing out these to ensure adjacent properties are not undermined. If suited to the new proposed development, it may well be worth leaving the Kennet Centre foundations in place to avoid such effects.

Except for the relocated substation adjacent to the ramp on Market St from the information available to RBG there was no indication that within the site footprint there are any other third-party assets or easements that would require special consideration during demolition.

The B1 and B2 interfaces relating to the multi storey car park vary significantly. The B2 interface running North-South between the retained 3 storey car park and the car deck on top the supermarket unit is clearly separated by a movement joint. This would infer that all the structure along this interface could be safely demolished without affecting the stability of the retained multi-storey car park, but this would need to be confirmed by further inspection.

The B1 interface will require further investigation to understand the interaction with the retained multi-storey car park. As the multi storey car park is 3 storeys and the adjoining structure is only single storey it is highly likely that they are structurally independent.

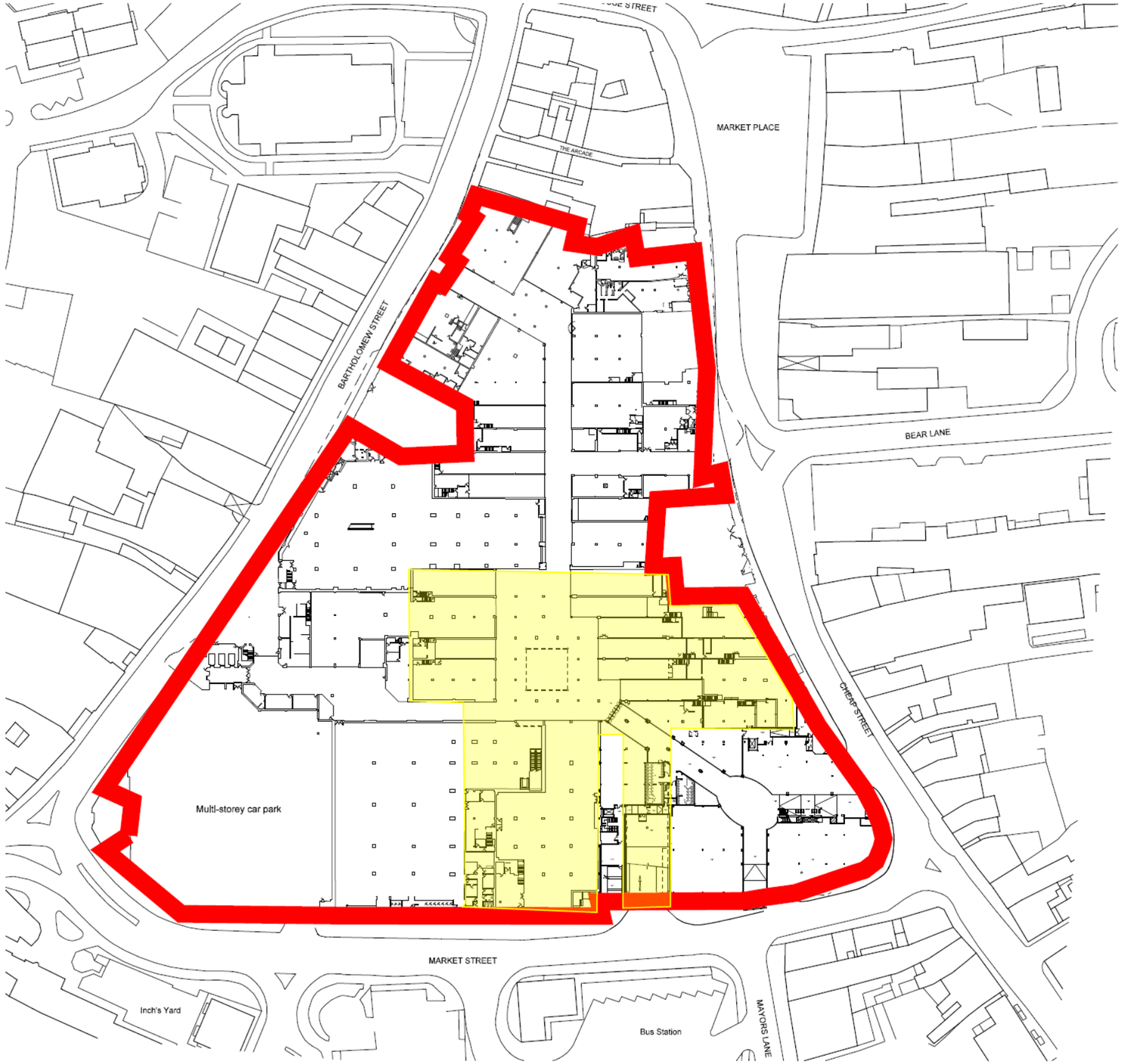
The C1 and C2 interfaces relating to the retained Cinema buildings are complex in that the Phase 4 development is intertwined with the Phase 1 development. We've identified that fortunately the structure supporting the cinema would appear to be generally structurally independent in terms of global stability. RBG recommend further investigative works should be carried out to determine an optimal demolition line that would optimise the development opportunity whilst retaining a functioning cinema.

Cinemas are inherently susceptible to the effects of noise and vibration; the demolition strategy should be developed in a manner that will retain full functionality of the cinema complex during demolition.

In summary RBG's desk study would conclude that following favourable investigative works relating to the C2 and B1 interfaces a demolition strategy could be developed that would allow the safe demolition of the 'Kennet Centre' without negatively impacting the structural stability of retained structures.

Appendix A

Timeline Sketches



KEY:

Phase 1
Phase 2
Phase 3
Phase 4

Structural, Civil & Construction Engineering Consultant
RobertBirdGroup
Member of the Surbana Jurong Group
LONDON OFFICE
Robert Bird & Partners Limited
Harling House
47-51 Great Suffolk Street
Southwark
London SE1 0BS, ENGLAND
Ph: +44 (0) 20 7633 2880
Email: enquiries@robertbird.com.au
Web: www.robertbird.com
UK Co. #447 2743

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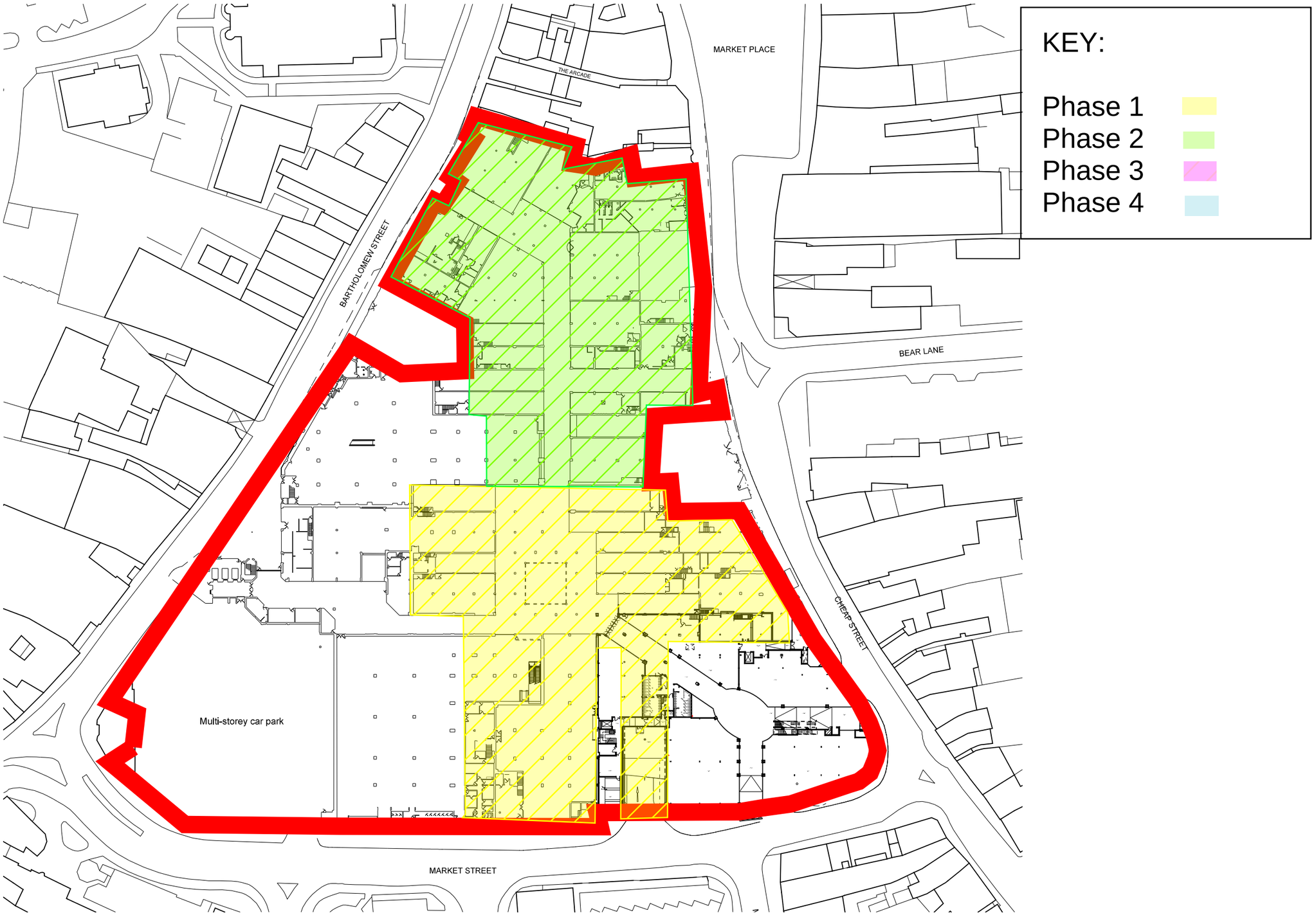
Title: Timeline Sketches	Scale at A3: NTS	Date: 04/10/2019		Rev: P01



Structural, Civil & Construction Engineering Consultant
RobertBirdGroup
Member of the Surbana Jurong Group
LONDON OFFICE
Robert Bird & Partners Limited
Harling House
47-51 Great Suffolk Street
Southwark
London SE1 0BS, ENGLAND
Ph: +44 (0) 20 7633 2880
Email: enquiries@robertbird.com.au
Web: www.robertbird.com
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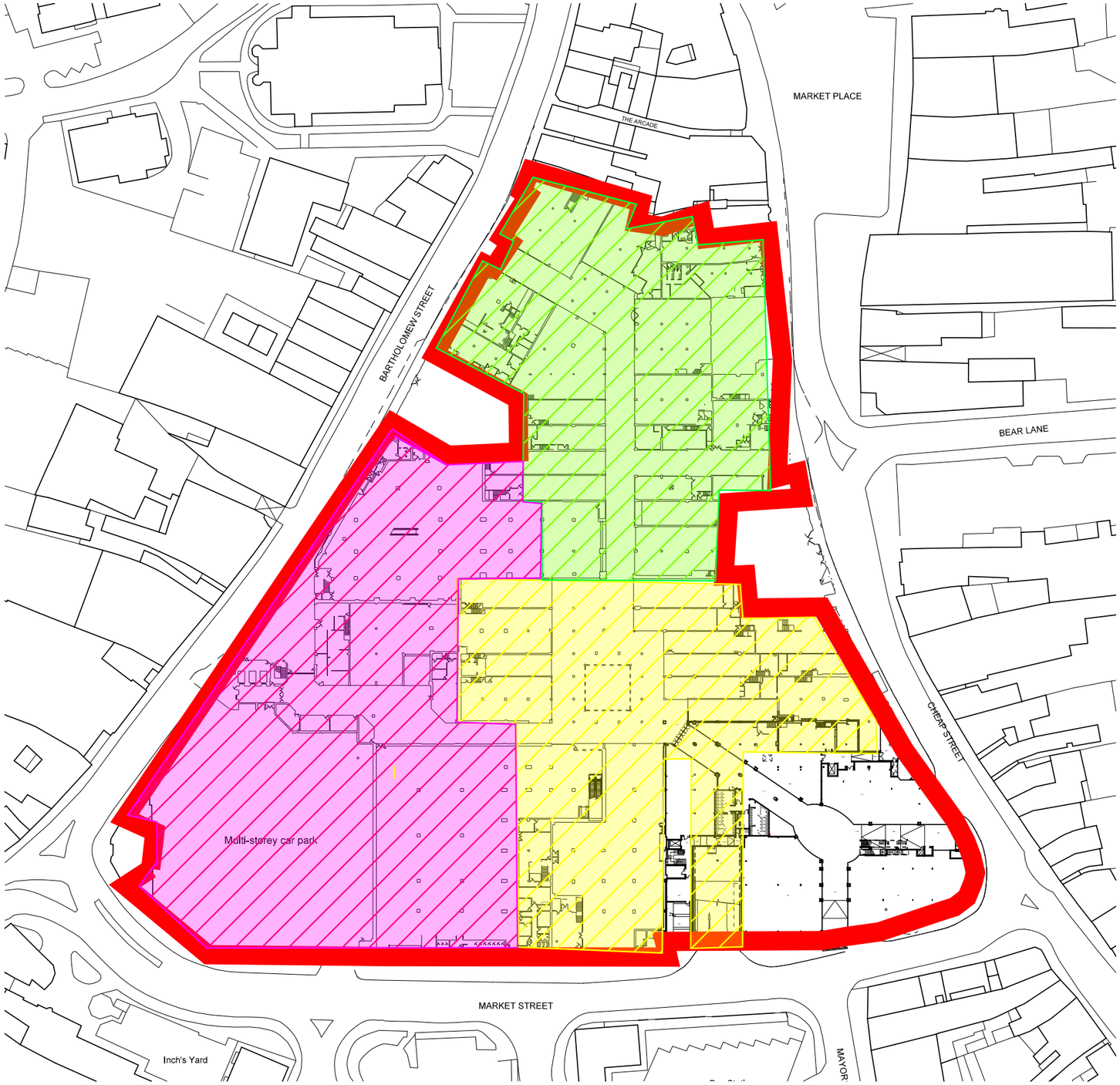

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Structural, Civil & Construction Engineering Consultant
RobertBirdGroup
Member of the Surbana Jurong Group
LONDON OFFICE
Robert Bird & Partners Limited
Harling House
47-51 Great Suffolk Street
Southwark
London SE1 0BS, ENGLAND
Ph: +44 (0) 20 7633 2880
Email: enquiries@robertbird.com.au
Web: www.robertbird.com
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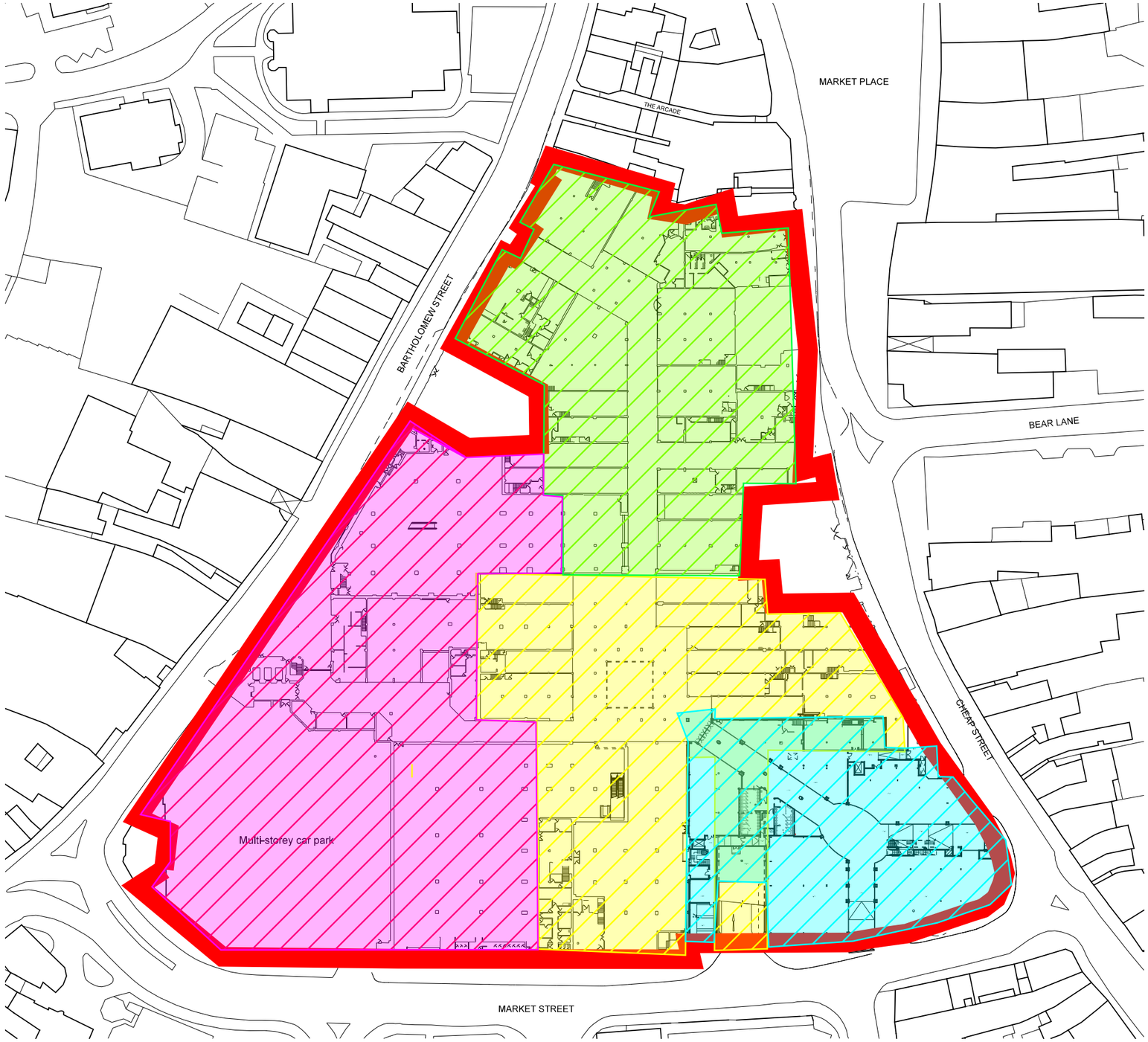

Structural, Civil & Construction Engineering Consultant
RobertBirdGroup
Member of the Surbana Jurong Group
LONDON OFFICE
Robert Bird & Partners Limited
Harling House
47-51 Great Suffolk Street
Southwark
London SE1 0BS, ENGLAND
Ph: +44 (0) 20 7633 2880
Email: enquiries@robertbird.com.au
Web: www.robertbird.com
UK Co. #447 2743

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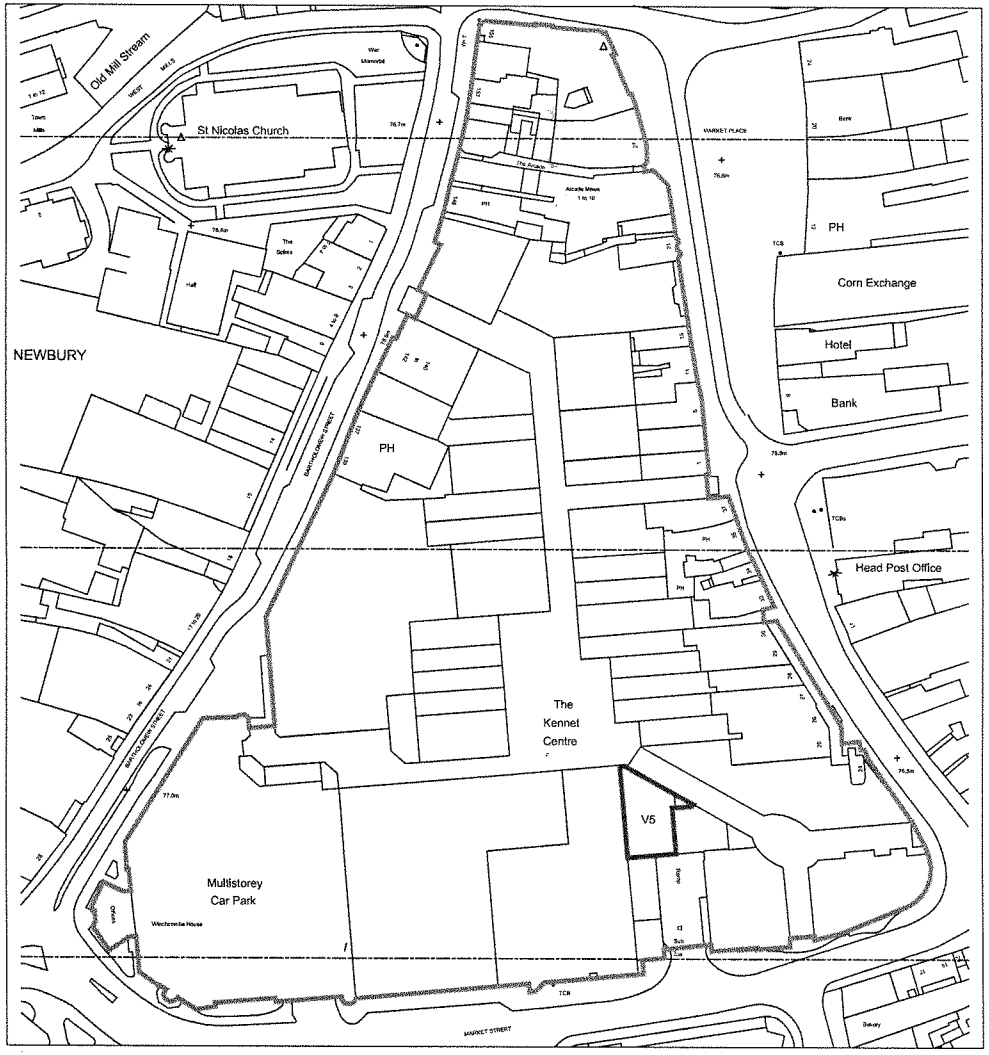
Appendix B
Historic Drawings

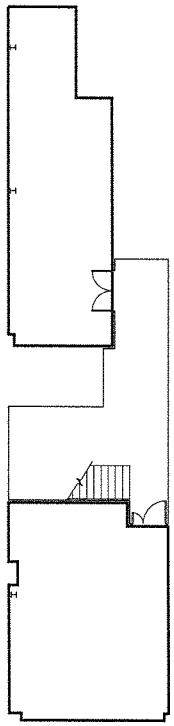


0m 50m 100m

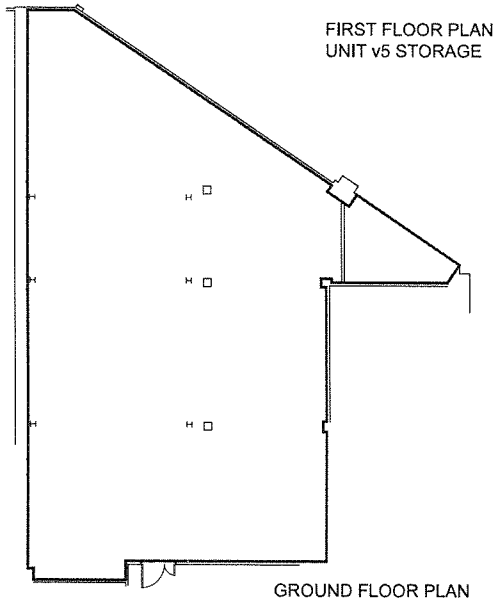
Site Location Plan
Scale 1:1250

DEMISE OF UNIT v5 THE KENNET CENTRE

LANDLORD OWNERSHIP



FIRST FLOOR PLAN
UNIT v5 STORAGE



GROUND FLOOR PLAN
UNIT v5

0m Floor Plan 10m
Scale 1:200

NOTES

1. No dimensions are to be scaled from this drawing.
2. Contractors must verify all figured dimensions on site before commencing any work or making any shop drawings.
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Rev	Description	By	Date

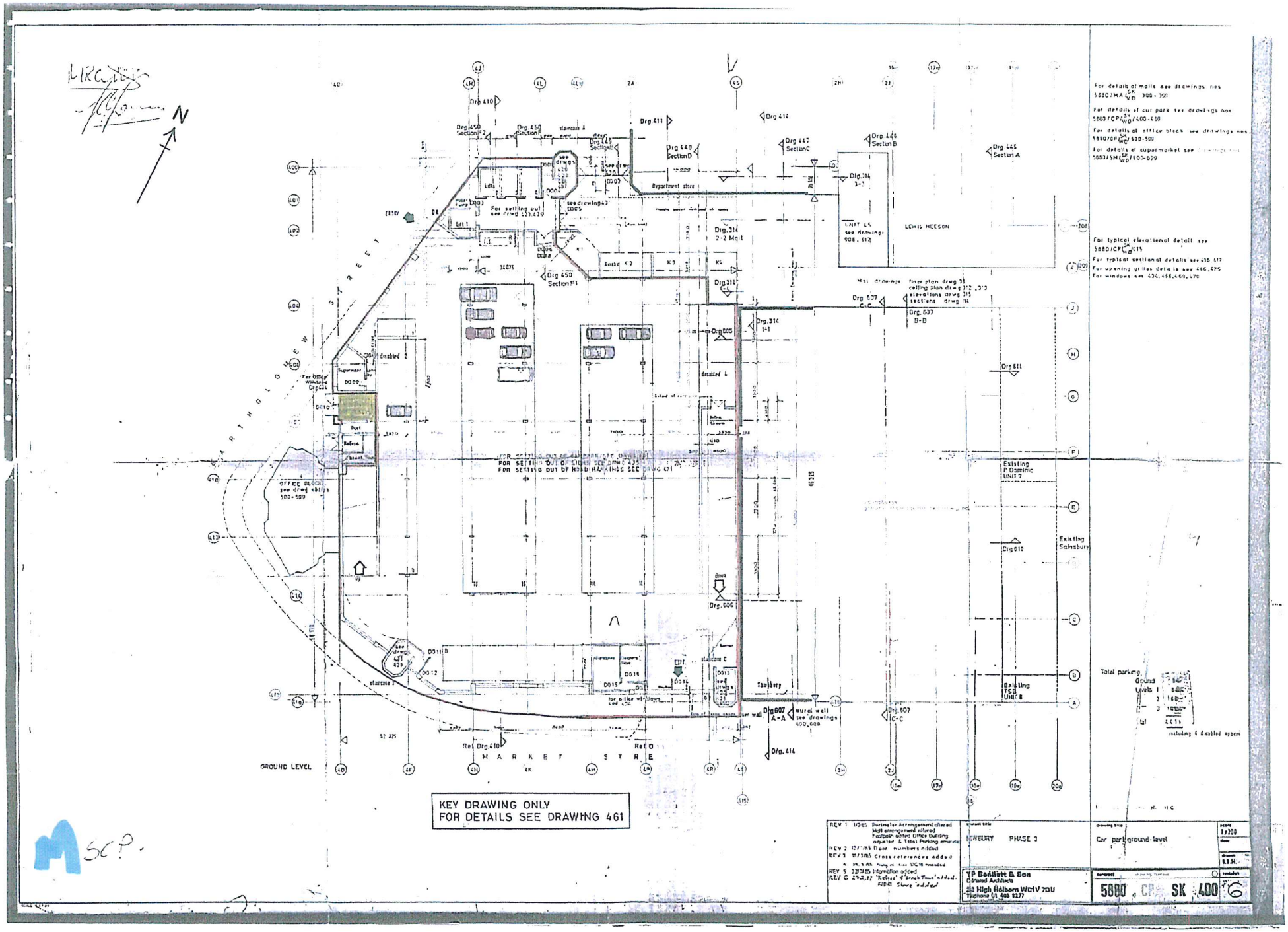
RHM Planning & Design Services
19 Russell Grove
Westbury Park
BRISTOL BS6 7UD
Tel: + 044 (0)7810 821409
Email: rhmplanning@mail.com
www.rhmplanning.co.uk

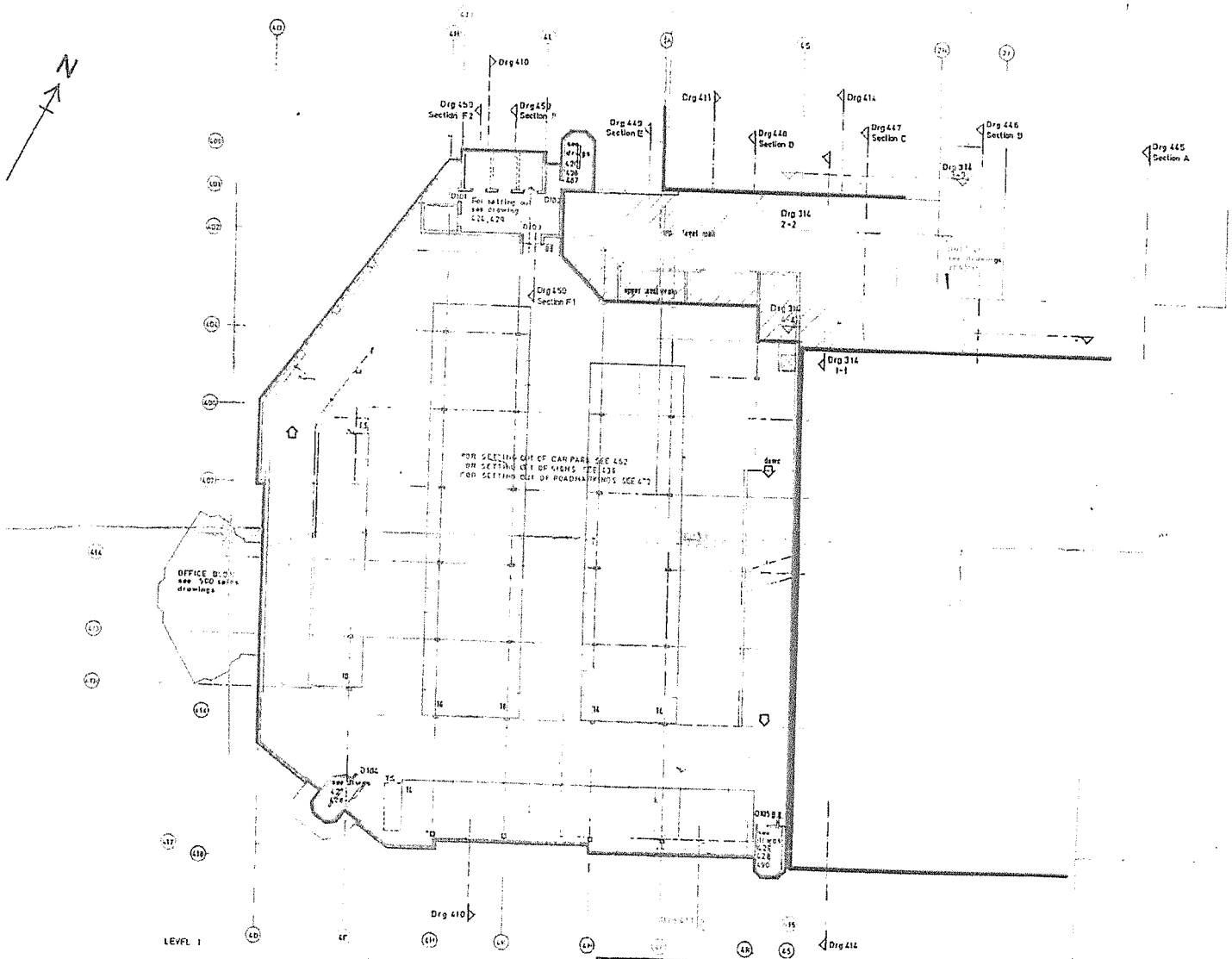
Client

Project
UNIT v5
THE KENNET CENTRE
NEWBURY

Drawing Title
LEASE PLAN

Drawing Number	Rev.	Scale
P0175 - 001		As Shown@A3
		Date OCT 2013


N
MARKET STREET
GROUND LEVEL
OFFICE BLOCK
see drwg nos
500-509
Existing
P Dominic
UNIT 7
Existing
Sainsbury
Existing
TSB
UNIT 8
LEWIS HEESON
Department store
Drg 410
Drg 411
Drg 414
Drg 445
Drg 446
Drg 447
Drg 448
Drg 450
Drg 606
Drg 607
Drg 610
Drg 611
Drg 807
KEY DRAWING ONLY
FOR DETAILS SEE DRAWING 461
NEWBURY PHASE 3
Car park ground level
1:200
5880 CP SK 400 6
TP Bennett & Son
Chrand Architects
32 High Holborn WC1V 7DU
Telephone 01 405 8277



FOR DETAILS OF HALLS SEE DRAWING NOS 5883/HAL/(P)/300-399

FOR DETAILS OF CARPARK SEE DRAWING NOS 5880/CP/(P)/400-429

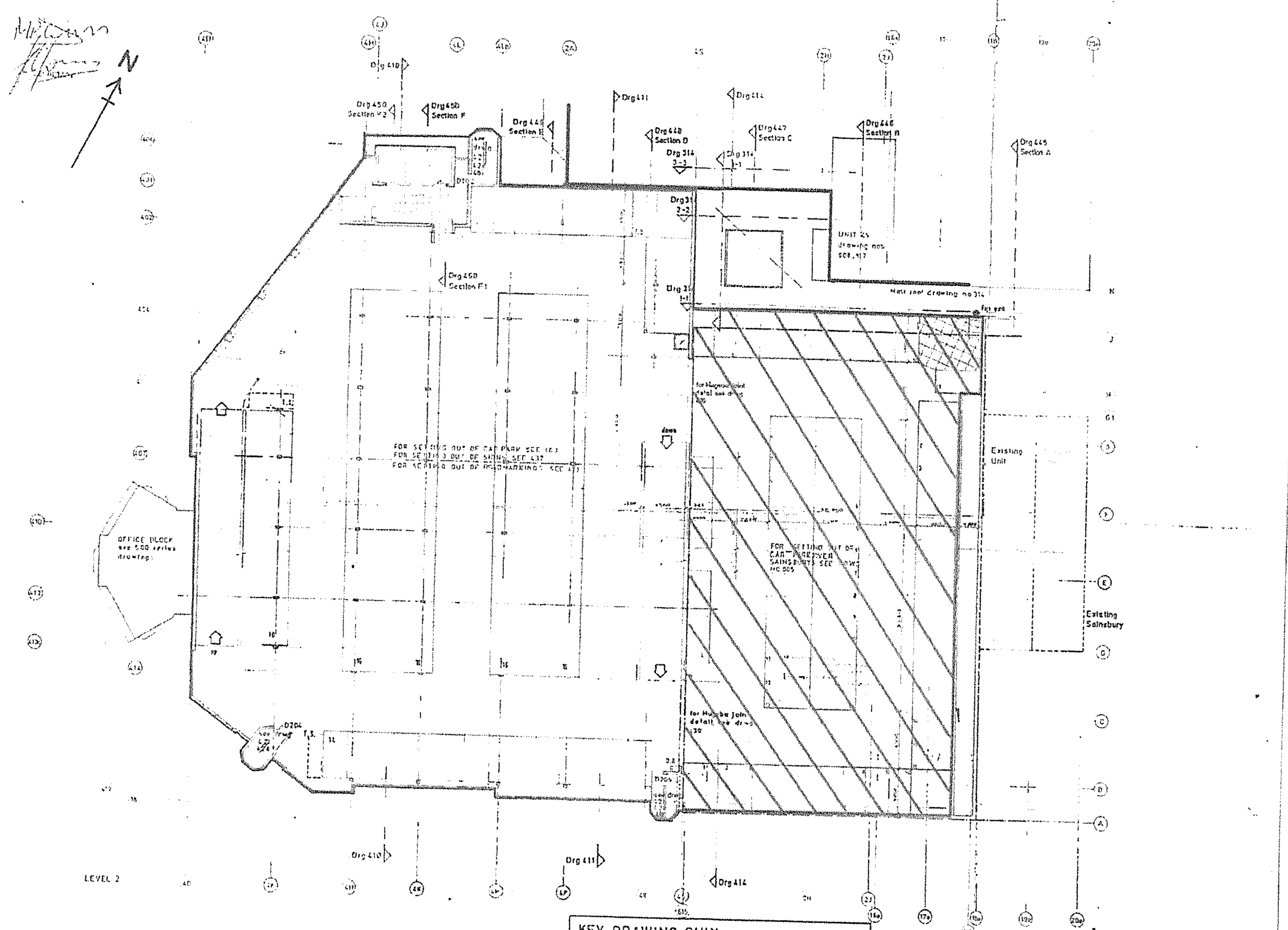
FOR DETAILS OF OFFICE BLOCK SEE DRAWING NOS 5880/OB/(P)/500-569

FOR DETAILS OF SUPERMARKET SEE DRAWING NOS 5880/SM/(P)/600-639

FOR TYPICAL ELEVATIONAL DETAIL SEE 435
FOR TYPICAL SECTIONAL DETAILS SEE 436,437
FOR OPENING GRILLES DETAILS SEE 466,476
FOR WINDOWS SEE 430, 465, 469, 470

MSCP

REV 1 10/05 Perimeter Arrangement altered and A/W Trucking added to			
REV 2 17/305 Door numbers added			
REV 3 18/305 Cross references added			
REV 4 27/705 Information added			
	NEWBURY PHASE 3	Car park level 1	scale 1:200
	T P Bennett & Son Chartered Architects 52 High Holborn WC1V 7DU Telephone 01 405 8277		drawn A.D.H.
		5880 CP SK 401	16



KEY DRAWING ONLY
FOR DETAILS SEE DRAWINGS 463 & 605

FOR DETAILS OF HALLS SEE DRAWING NOS. 5800/HA/SK/WD/300-399
FOR DETAILS OF CAR PARK SEE DRAWINGS NOS 5800/CP/SK/WD/400-499
FOR DETAILS OF OFFICE BLOCK 5800/OC/SK/WD/500-599
FOR DETAILS OF SUPERMARKET SEE DRAWING NOS 5800/SM/SK/WD/600-699

FOR TYPICAL ELEVATIONAL DETAIL SEE 415
FOR TYPICAL SECTIONAL DETAILS SEE 416,417
FOR OPENING GRILLES DETAILS SEE 416,475
FOR WINDOWS SEE 434,468,469,470

REV 1 12.85 Perimeter Arrangement altered and A/V Trunking Indicated
REV 2 12.2.85 Door numbers added
REV 3 18.3.85 Cross references added
REV 4 24.5.85 Minor amendments to parking layout adj to fire exit
REV 5 12.7.85 Information added

Drawing no. formerly 8

KENNET CENTRE
NEWBURY PHASE 3

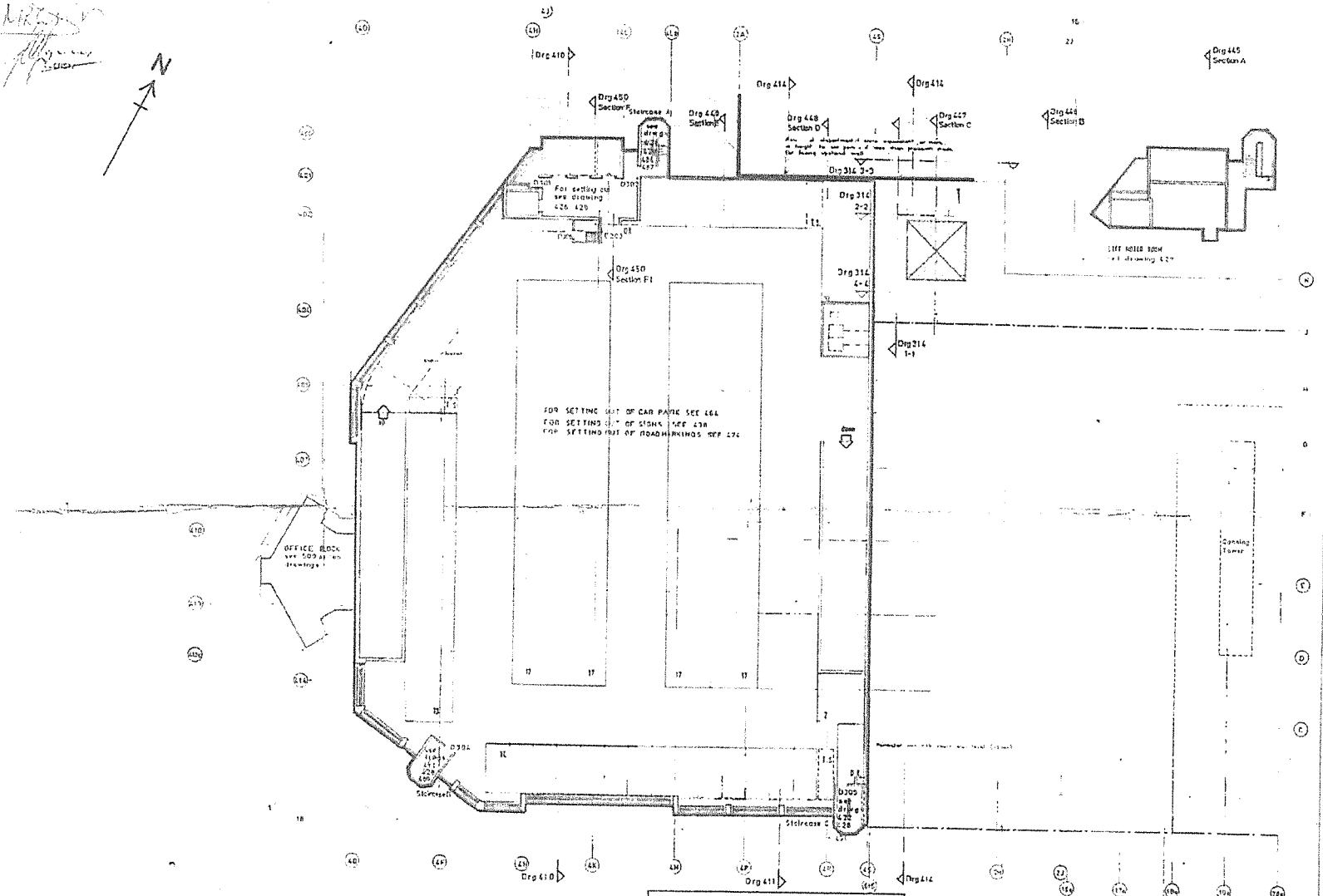
Car park level 2

Scale 1:200

Drawn E.D.M.

T P Bennett & Son
Chartered Architects
202 High Holborn WC1V 7DU
Telephone 01 405 9277

5800 CP SK 402/5



FOR DETAILS OF MALLS SEE DRWG NOS SK(00)MALL 300 - 399
FOR DETAILS OF CAR PARK SEE DRWGS NOS SK(00)CP(LEVEL) 400 - 499
FOR DETAILS OF OFFICE BLOCK SEE DRWGS NOS SK(00)OFFICE 500 - 599
FOR DETAILS OF SUPERMARKET SEE DRWG NOS SK(00)SM 600 - 699

FOR TYPICAL ELEVATIONAL DETAIL SEE 475
FOR TYPICAL SECTIONAL DETAILS SEE 476, 477
FOR OPENING SASHES DETAILS SEE 464, 475
FOR WINDOWS SEE 476, 478, 479, 480

MSCP

REV. 1 2.2.85 Perimeter Arrangement altered and A/W, Trunking indicated and Lift Motor Room altered
REV 2 12.2.85 Door numbers and stair to Lift motor room added
REV 3 18.3.85 Cross references added
REV 4 22.7.85 Information added

NEWBURY PHASE 3

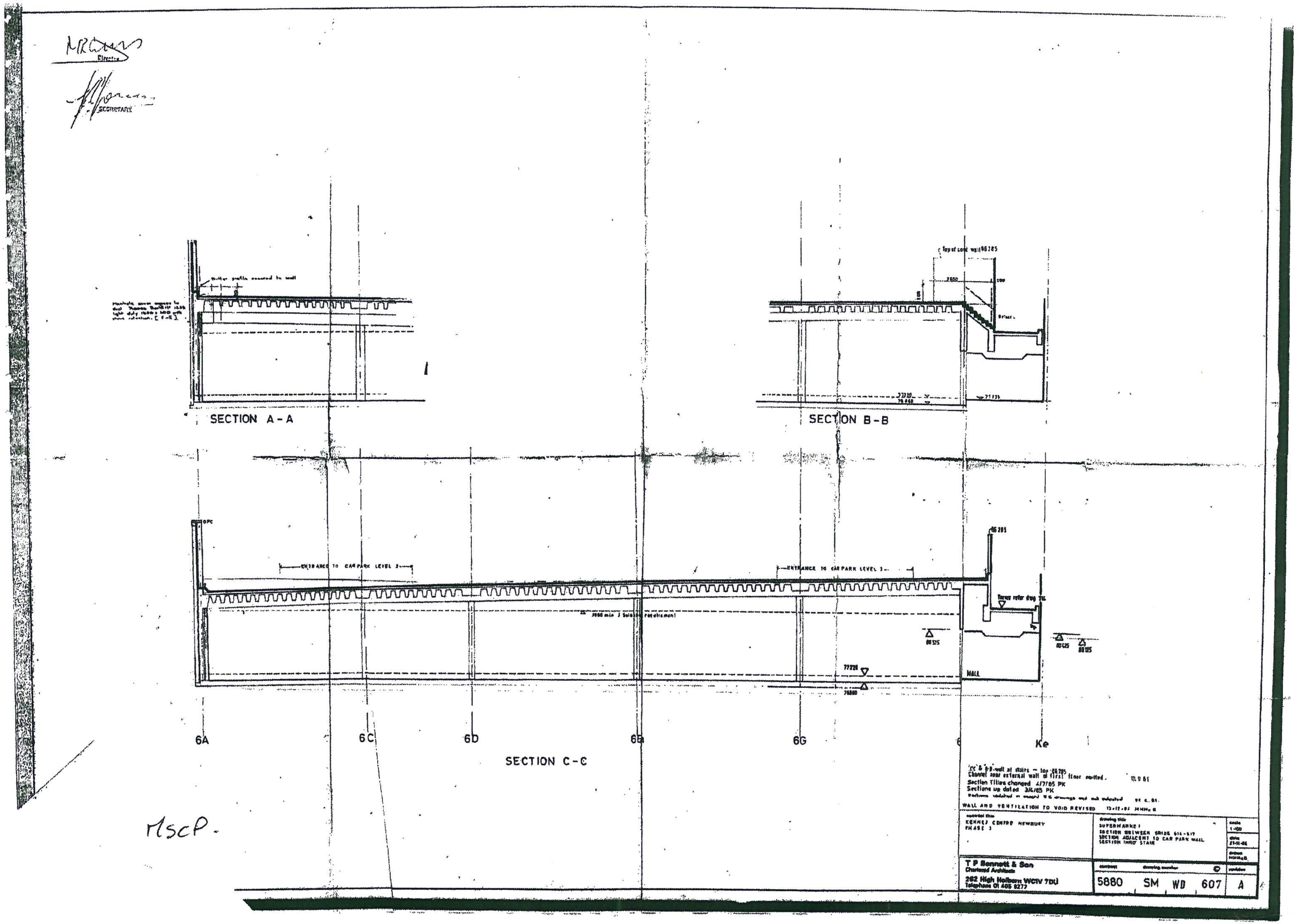
Car park level 3

1:200

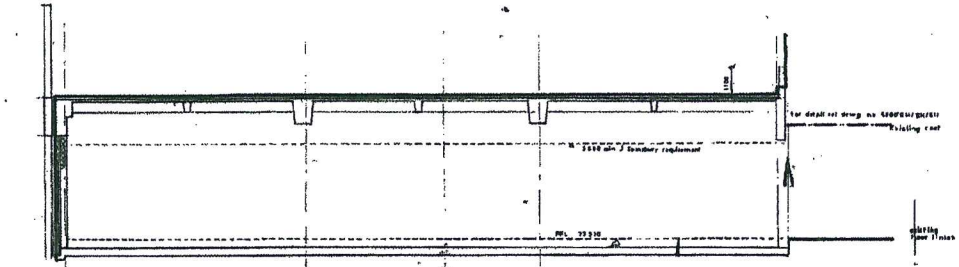
G.E.H.

T P Bennett & Son
Chartered Architects
32 High Holborn WC1V 7DU
Telephone 01 405 8277

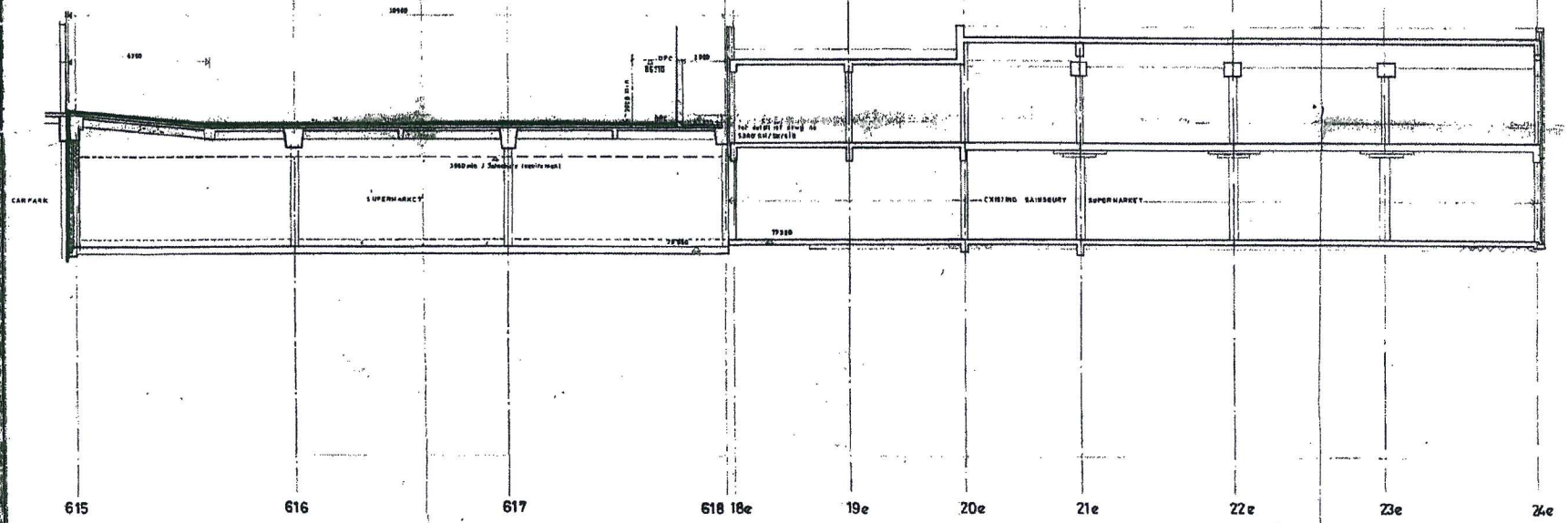
5880 CP SK 403 4


SECTION A - A
SECTION B - B
SECTION C - C
ENTRANCE TO CAR PARK LEVEL 3
6A
6C
6D
6G
Ke
WALL
Secretary
MSCP.
Section Tiiles changed 4/7/85 PK
Sections up dated 3/6/85 PK
WALL AND VENTILATION TO VOID REVISED
KENNET CENTRE NEWBURY
PHASE 1
SUPERMARKET
SECTION ADJACENT TO CAR PARK WALL
SECTION THRO' STAIR
T P Bennett & Son
Chartered Architects
202 High Holborn WC1V 7DU
5880 SM WD 607
A

MSCP.



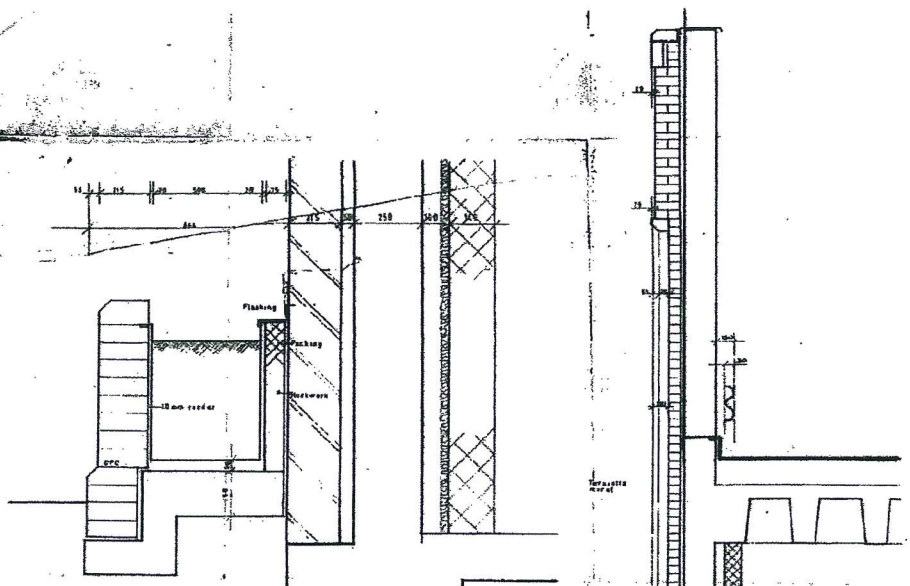
SECTION BETWEEN GRIDS 6G&6J



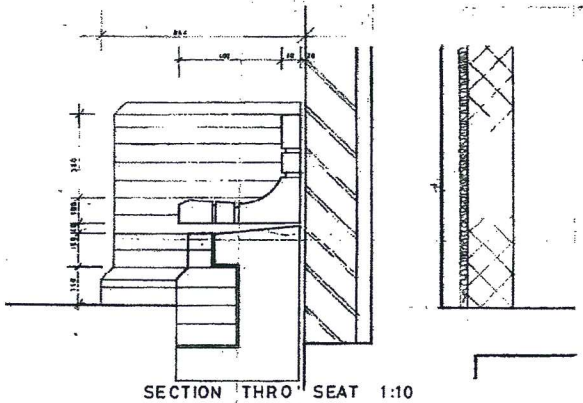
SECTION ON GRID 6C

contract title: BENNET CENTRE NEWBURY PHASE 2	drawing title: SUPERMARKET SECTION ON GRID 6C SECTION BETWEEN GRIDS 6G & 6J
T P Bennett & Son Chartered Architects 242 High Holborn WC1V 7DU Telephone 01 405 9277	contract: 5880 drawing number: SM WD 606 revision: A

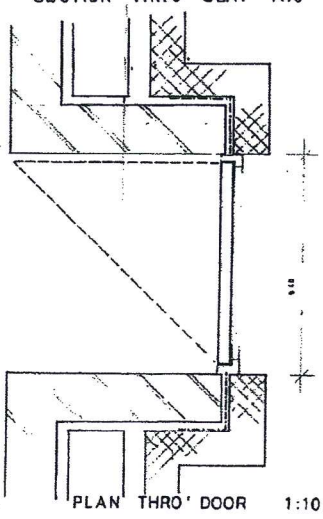
15



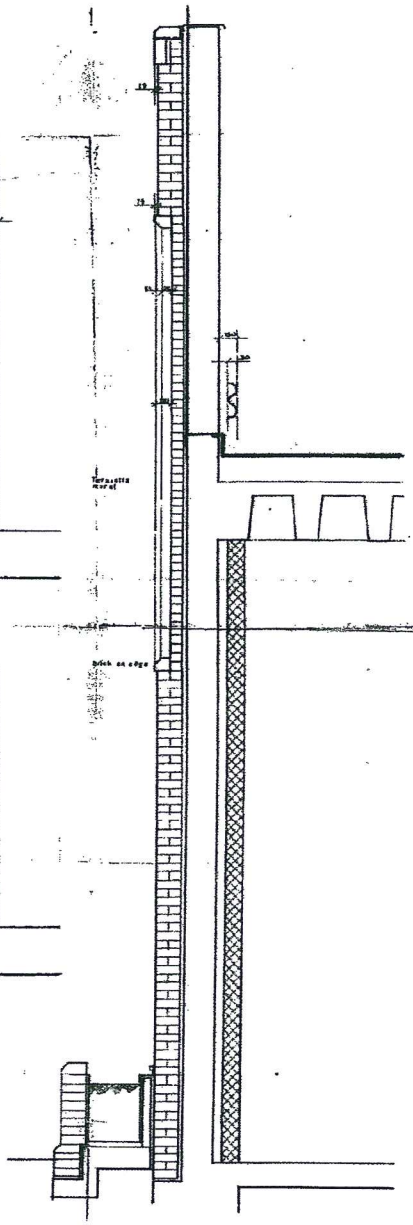
SECTION THRO' PLANTER 1:10



SECTION THRO' SEAT 1:10



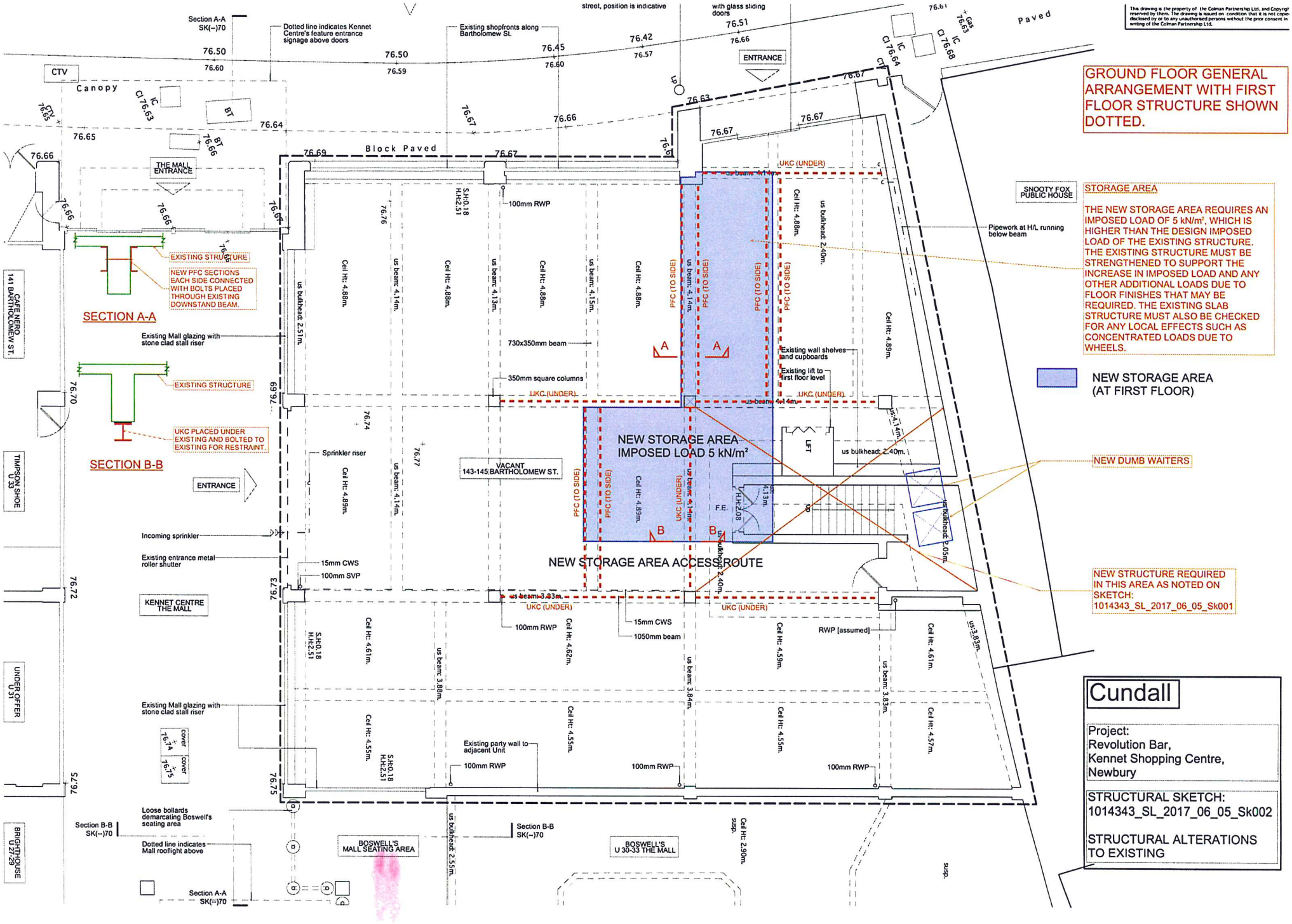
PLAN THRO' DOOR 1:10



1:20
SECTION THRO' MURAL WALL

MSCP

T P Bennett & Son	KENNET CENTRE NEWBURY PHASE 3
5880' SM SK 613	SECTIONS AND DETAILS THRO MURAL WALL


GROUND FLOOR GENERAL ARRANGEMENT WITH FIRST FLOOR STRUCTURE SHOWN DOTTED.
STORAGE AREA
THE NEW STORAGE AREA REQUIRES AN IMPOSED LOAD OF 5 kN/m², WHICH IS HIGHER THAN THE DESIGN IMPOSED LOAD OF THE EXISTING STRUCTURE. THE EXISTING STRUCTURE MUST BE STRENGTHENED TO SUPPORT THE INCREASE IN IMPOSED LOAD AND ANY OTHER ADDITIONAL LOADS DUE TO FLOOR FINISHES THAT MAY BE REQUIRED. THE EXISTING SLAB STRUCTURE MUST ALSO BE CHECKED FOR ANY LOCAL EFFECTS SUCH AS CONCENTRATED LOADS DUE TO WHEELS.
NEW STORAGE AREA (AT FIRST FLOOR)
NEW DUMB WAITERS
NEW STRUCTURE REQUIRED IN THIS AREA AS NOTED ON SKETCH: 1014343_SL_2017_06_05_Sk001
NEW STORAGE AREA IMPOSED LOAD 5 kN/m²
NEW STORAGE AREA ACCESS ROUTE
SECTION A-A
SECTION B-B
EXISTING STRUCTURE
NEW PFC SECTIONS EACH SIDE CONNECTED WITH BOLTS PLACED THROUGH EXISTING DOWNSTAND BEAM.
UKC PLACED UNDER EXISTING AND BOLTED TO EXISTING FOR RESTRAINT.
Cundall
Project: Revolution Bar, Kennet Shopping Centre, Newbury
STRUCTURAL SKETCH: 1014343_SL_2017_06_05_Sk002
STRUCTURAL ALTERATIONS TO EXISTING
SNOOTY FOX PUBLIC HOUSE
CAFE NERO 141 BARTHOLOMEW ST.
TIMPSON SHOE U 33
UNDER OFFER U 31
BRIGHTHOUSE U 27-29
KENNET CENTRE THE MALL
VACANT 143-145 BARTHOLOMEW ST.
BOSWELL'S MALL SEATING AREA
BOSWELL'S U 30-33 THE MALL
THE MALL ENTRANCE
ENTRANCE
Canopy
Block Paved
Paved
Existing Mall glazing with stone clad stall riser
Existing entrance metal roller shutter
Sprinkler riser
Incoming sprinkler
Loose bollards demarcating Boswell's seating area
Dotted line indicates Mall rooflight above
Dotted line indicates Kennet Centre's feature entrance signage above doors
Existing shopfronts along Bartholomew St
Pipework at H/L running below beam
Existing wall shelves and cupboards
Existing lift to first floor level
730x350mm beam
350mm square columns
100mm RWP
15mm CWS
100mm SVP
1050mm beam
RWP [assumed]
Existing party wall to adjacent Unit
UKC (UNDER)
PFC (TO SIDE)
Ceil Ht: 4.88m.
Ceil Ht: 4.89m.
Ceil Ht: 4.61m.
Ceil Ht: 4.62m.
Ceil Ht: 4.59m.
Ceil Ht: 4.55m.
Ceil Ht: 4.57m.
us bulkhead: 2.40m.
us bulkhead: 2.51m.
us bulkhead: 2.55m.
us beam: 4.14m.
us beam: 3.83m.
us beam: 3.88m.
us beam: 3.84m.
F.E.
LIFT
susp.
Section A-A SK(--)70
Section B-B SK(--)70
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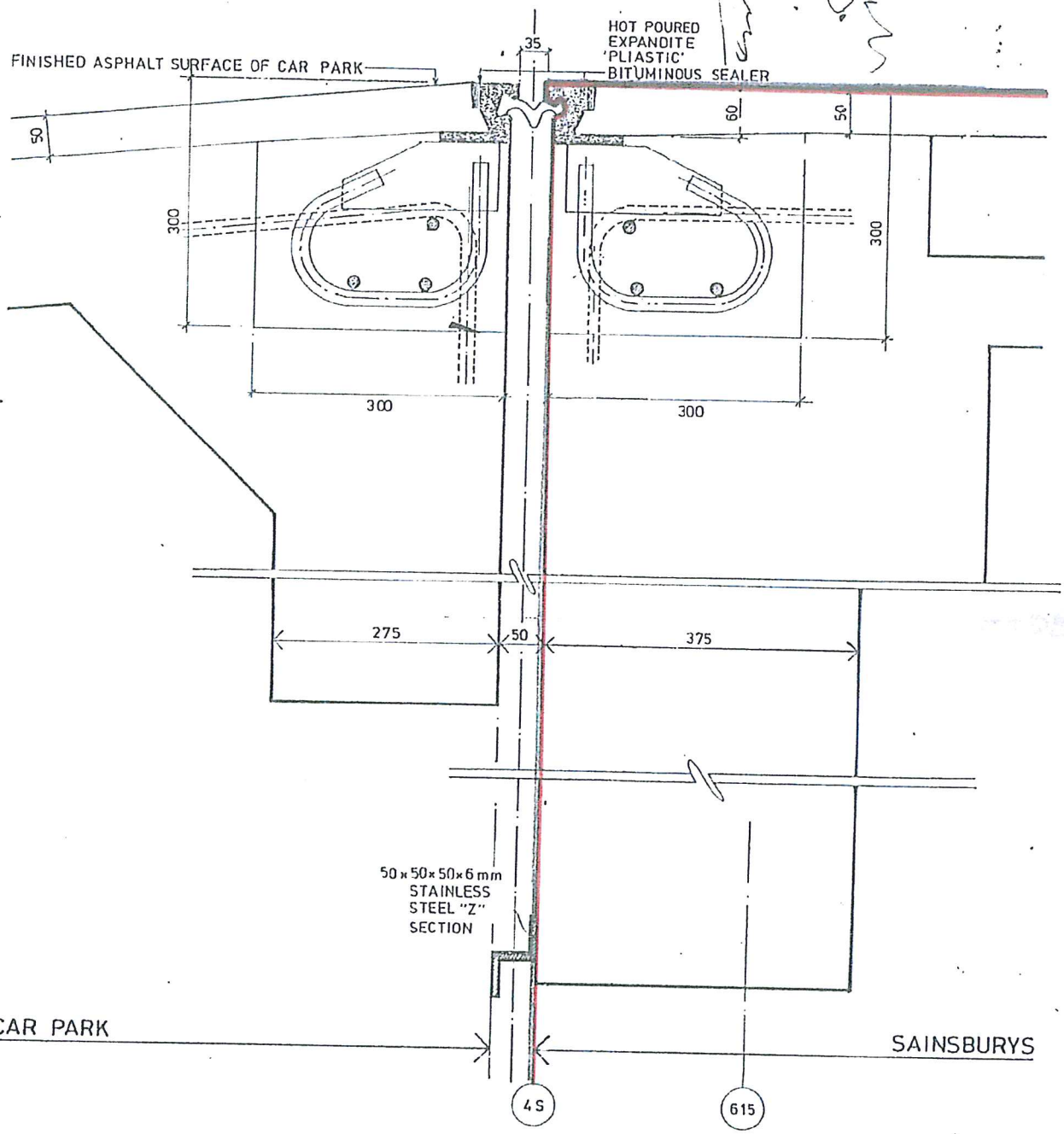


REV D 24/7/85 Detail amended

MScP

contract title
KENNET CENTRE
NEWBURY PHASE 3

drawing title
MAGEBA JOINT CONSTRUCTION
LEVEL 2 CAR PARK / SAINSBURY ROOF

scale 1:5

date APR. 85

drawn P K

T P Bennett & Son
Chartered Architects
262 High Holborn WC1V 7DU
Telephone 01 405 9277

contract	drawing no. type	©	revision
5880	CP SK 439		D

SCALE REPRO 6557/84

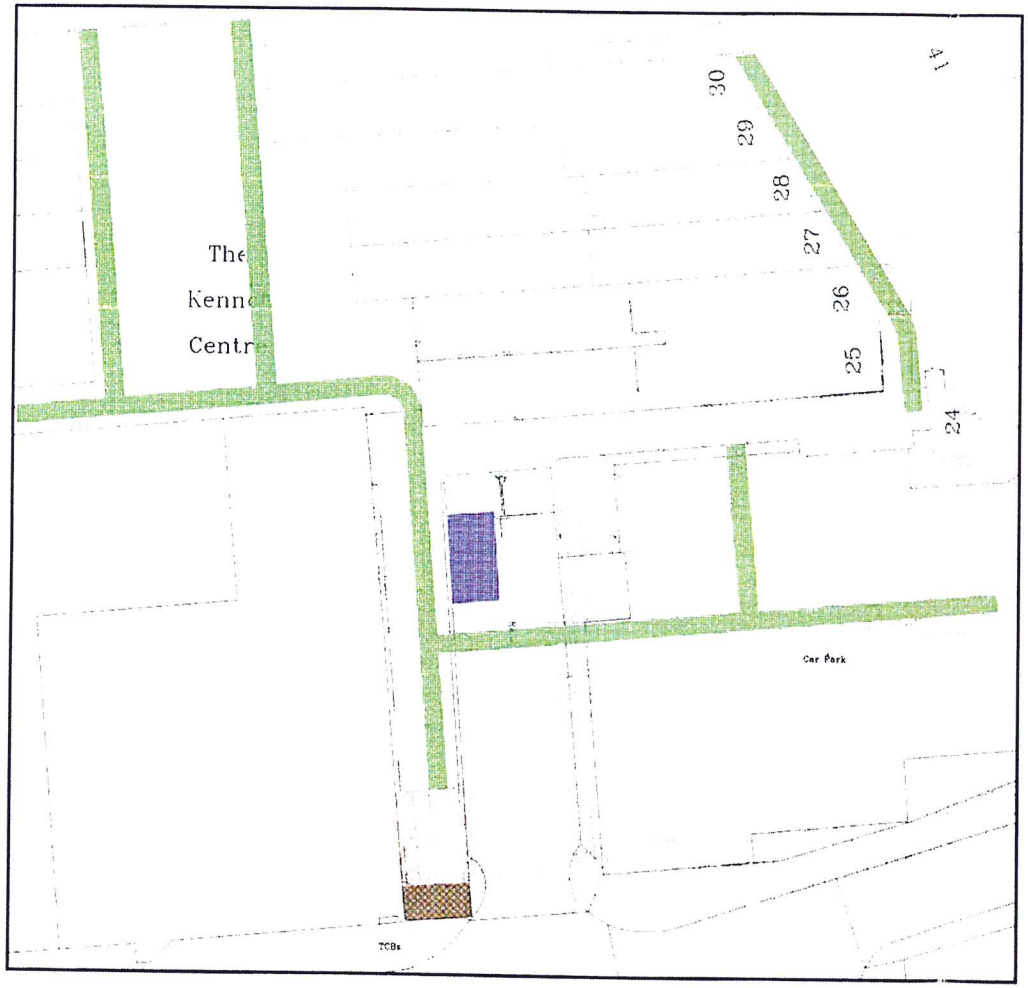
2009.

PLAN NO 1

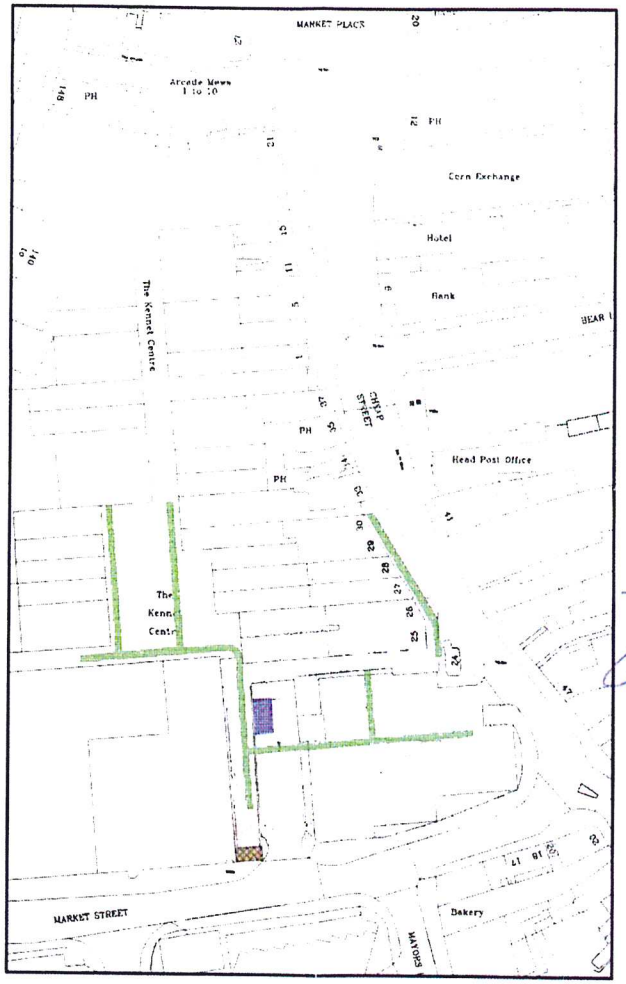
A Revised 03/06/2009

B Revised 07/07/2009

Site Plan .500



Location @ 1 1250



JEHCB1

Substation shown PINK
Cable Easements shown GREEN
Access shown BROWN
Access with cables shown BROWN HATCHED BLACK
Land to be relinquished shown BLUE

Scottish and Southern Energy plc
Registered in Scotland No 117119
Registered Office: Inveralmond House
200 Dunkeld Road
Perth, PH1 3AQ

Scottish and Southern Energy plc

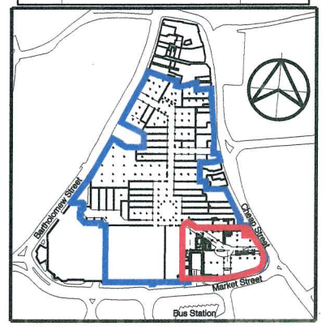
Newbury Cinema Complex
Market Place
Newbury
PRN-28525

Grid Ref: SU4716-6690
Drawing Ref: READ_073B
Date: 07/07/2009

Dimensions are not to be scaled from this drawing.

This drawing is based on current available information and should not be relied on as accurate measurements of usable space.

Revision	Notes	Date
L	General amendment, amendment to lift shafts, ramp added to comply with change in levels.	11/03/08
M	Minor adjustments Wall on gridline 2 moved by 400mm	09/04/08
N	Paving re-hatch; Electric cupboard shown; Riser to Unit 2 enlarged; Rearranged north fire exit corridor; Screet hatched,	13/05/08
O	Minor adjustments	13/06/08
P	General amendments	22/09/08
Q	Existing MH added and walls amended	01/10/08
R	New construction around existing MH amended following discussions on site	09/10/08
S	Drawing amended following comments received from the Structural Engineer	28/10/08
T	Alterations at infill building and beneath ramp	12/12/08
U	Alterations near D0/19	06/01/09
V	Columns co-ordinated with latest ABA plans	20/01/09
W	Setting out of Mall 1	27/02/09
X	Block movement joints amended	24/03/09



BUILDERS SISK CONTRACTORS

Established 1859

CONSTRUCTION ISSUE

colman architects

Project

Proposed Cinema Development
The Kennet Centre
Newbury

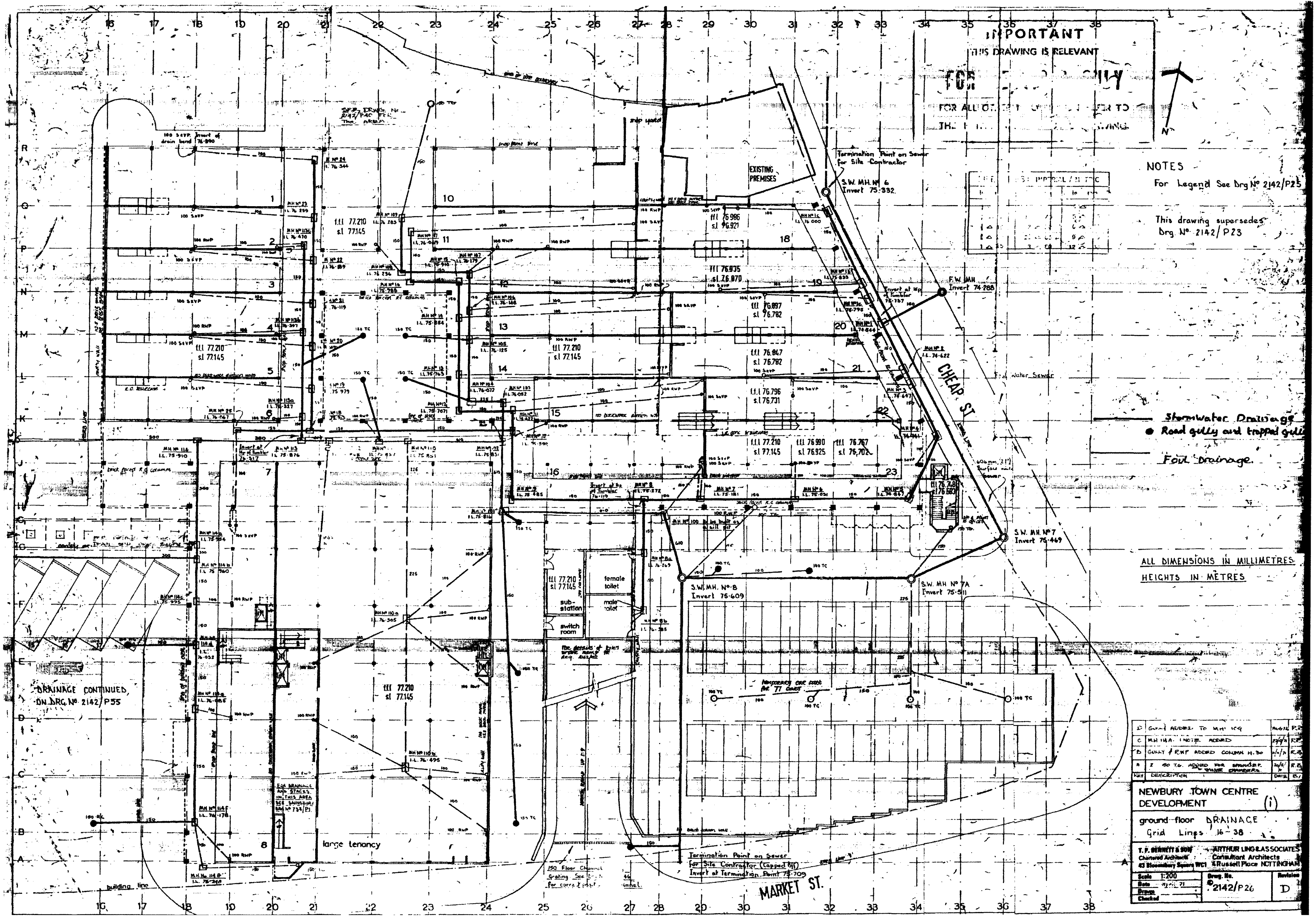
Title

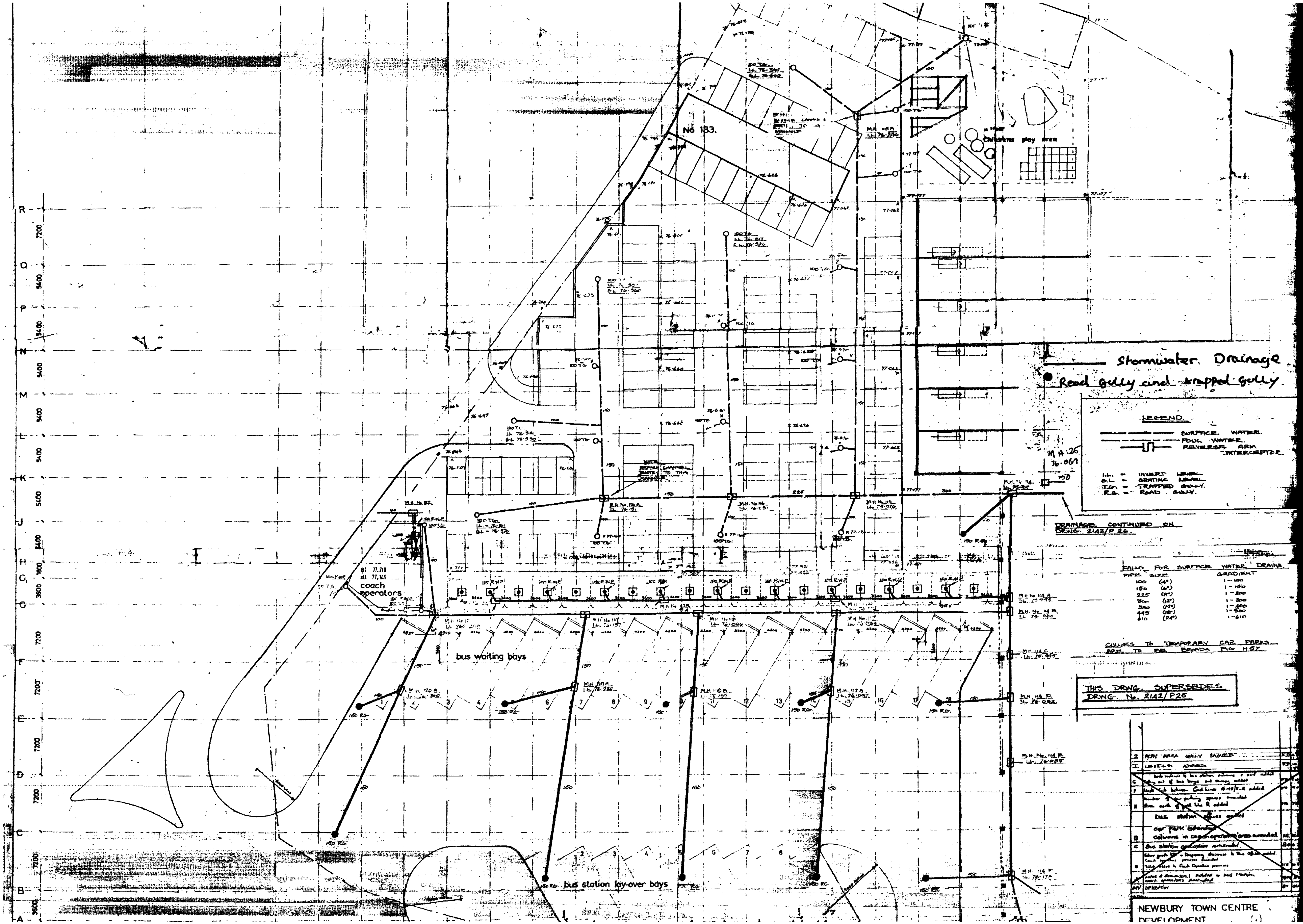
Proposed Ground Floor Plan

Scale		Date
1:125 @A1	1:250 @A3	20/03/2007
Drawn by: ADS	Checked by Proj. Arch.: CP	Checked By Manager
Drawing No.: 2572/L(--)101		Revision: X

The Colman Partnership Limited
27 Harcourt Street
London
W1H 4HP
Telephone : 020 7535 2040
Facsimile : 020 7535 2041
email: projects@colmanarchitects.co.uk
www.colmanarchitects.co.uk

CAD Filename: \\Tcp1\architects\TCP Jobs\2572\2 SHTS\L(--)\L(--)101.dgn
24/03/2009


IMPORTANT
THIS DRAWING IS RELEVANT
NOTES
For Legend See Drg Nº 2142/P25
This drawing supersedes Drg. Nº 2142/P23
Stormwater Drainage
Road gully and trapped gully
Foul drainage.
ALL DIMENSIONS IN MILLIMETRES
HEIGHTS IN METRES
EXISTING PREMISES
Termination Point on Sewer for Site Contractor
S.W. MH. Nº 6 Invert 75·332
F.W. MH Invert 74·288
S.W. MH Nº 7 Invert 75·469
S.W. MH. Nº 8 Invert 75·409
S.W. MH Nº 7A Invert 75·511
CHEAP ST
MARKET ST.
DRAINAGE CONTINUED ON DRG Nº 2142/P55
large tenancy
sub-station
switch room
female toilet
male toilet
building line
Termination Point on Sewer for Site Contractor (capped off) Invert at Termination Point 75·709
NEWBURY TOWN CENTRE DEVELOPMENT
ground floor DRAINAGE Grid Lines 16 - 38
T. P. BENNETT & SON Chartered Architects 43 Bloomsbury Square WC1
ARTHUR LING & ASSOCIATES Consultant Architects 4 Russell Place NOTTINGHAM
Scale 1:200
Drwg. No. 2142/P26
Revision D



No 133.
Childrens play area
Stormwater Drainage
Road gully and trapped gully
LEGEND
SURFACE WATER
FOUL WATER
REVERSED ARM INTERCEPTOR
I.L. = INVERT LEVEL
G.L. = GRATING LEVEL
T.G. = TRAPPED GULLY
R.G. = ROAD GULLY
DRAINAGE CONTINUED ON DRWG. 2142/P 26.
FALLS FOR SURFACE WATER DRAINS
PIPE SIZE GRADIENT
100 (4") 1-100
150 (6") 1-150
225 (9") 1-200
300 (12") 1-300
380 (15") 1-400
445 (18") 1-500
610 (24") 1-610
THIS DRWG. SUPERSEDES DRWG. No. 2142/P25
coach operators
bus waiting bays
bus station lay-over bays
bus station offices moved
car park extended
Bus station cantilever amended
NEWBURY TOWN CENTRE DEVELOPMENT

DRAINAGE PLAN PHS2

IMPORTANT
THIS DRAWING IS
RELEVANT FOR SERVICES ONLY
FOR ALL OTHER INFORMATION REFER TO THE LATEST ARCHITECTURAL DRAWINGS

THE BRICKLAYERS ARMS

BARTHOLOMEW STREET

existing public house

hoist For Courage Ltd

(Phase 1)

FUTURE SITE

SHOP UNITS

45 44 43 42 41 40 39 38 37 36 35

24 25 26 27 29 33 34

28 30 31 32 11 13 15 21

existing shops

existing shop

existing public house

hoist For Courage Ltd

THE CATHERINE WHEEL

MARKET PLACE

NOTES.
(A) BOTTOM OF S.W. DRAIN = 75·767 – TOP OF F.W. DRAIN = 75·689 AT CROSS OVER POINT.
(B) BOTTOM OF F.W. DRAIN = 76·197 – TOP OF S.W. DRAIN = 76·047 AT CROSS OVER POINT.
(C) BOTTOM OF F.W. DRAIN = 76·342 – TOP OF S.W. DRAIN = 76·126 AT CROSS OVER POINT.

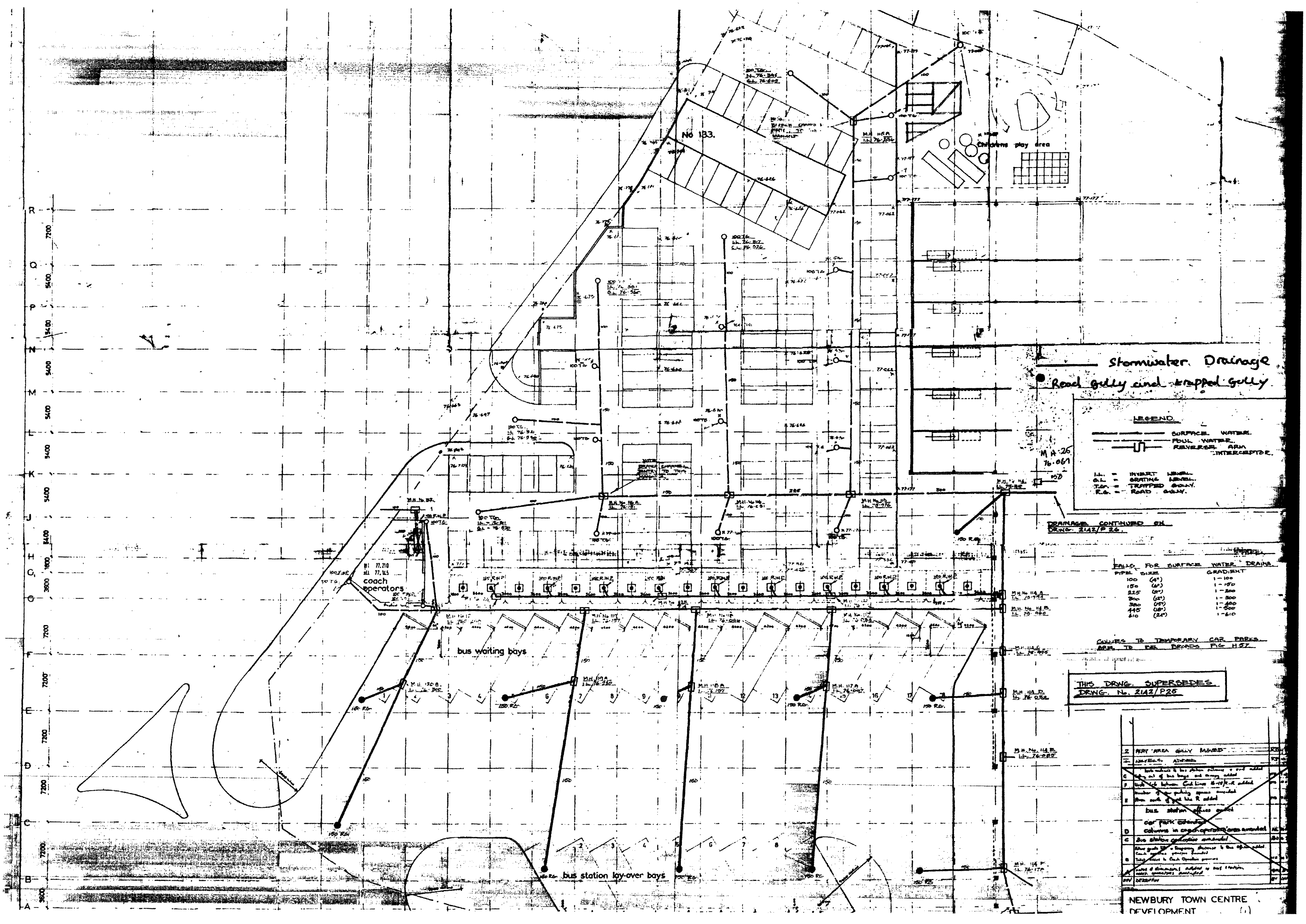
BASED ON DRWG. No: 4479 253 REV. D

REV C. – REVISIONS TO WHITE BOX – 28/3/81 – DRC BH – R.B.
REV. B. – RECEIVED – AUG. 81.

FOUL & SURFACE WATER DRAINAGE.

Rev E – revisions 18.2.81

contract title	NEWBURY TOWN CENTRE (Phase 2)
drawing title	Shopping Mall GROUND Floor Plan
scale	1:200
date	8.9.81
drawn	
T P Bennett & Son, Chartered Architects, 262 High Holborn WC1V 7DU, Telephone 01 405 9277	
contract	4479
drawing number	L.D.(52) 10
revision	C



No 183.
Childrens play area
coach operators
bus waiting bays
bus station lay-over bays
Stormwater Drainage
Road gully and trapped gully
LEGEND.
SURFACE WATER.
FOUL WATER.
REVERSE ARM INTERCEPTOR.
I.L. = INVERT LEVEL.
G.L. = GRATING LEVEL.
T.G. = TRAPPED GULLY.
R.G. = ROAD GULLY.
DRAINAGE CONTINUED ON DRWG. 2142/P 26.
FALLS FOR SURFACE WATER DRAINS
PIPE SIZE
GRADIENT
100 (4") 1 - 100
150 (6") 1 - 150
225 (9") 1 - 200
300 (12") 1 - 300
380 (15") 1 - 400
445 (18") 1 - 500
610 (24") 1 - 610
GULLIES TO TEMPORARY CAR PARKS.
ARE TO BE BROADS FIG H57
THIS DRWG. SUPERSEDES
DRWG. No. 2142/P25
2 ARMY AREA GULLY MOVED
1 LEVELS ADDED
bus station offices added
car park extended
D columns in coach operators area amended
C Bus station canopies amended
REV DESCRIPTION
NEWBURY TOWN CENTRE
DEVELOPMENT

THE BRICKLAYERS ARMS

existing public house

FUTURE SITE

STORAGE

STORAGE

STORAGE

existing kiosk

water storage tank and pumps for sprinkler system

Mall Below

Vehicular Access to Storage

STORAGE

Access Walkway

STORAGE

hoist

existing public house

Penn's

THE CATHERINE WHEEL

existing

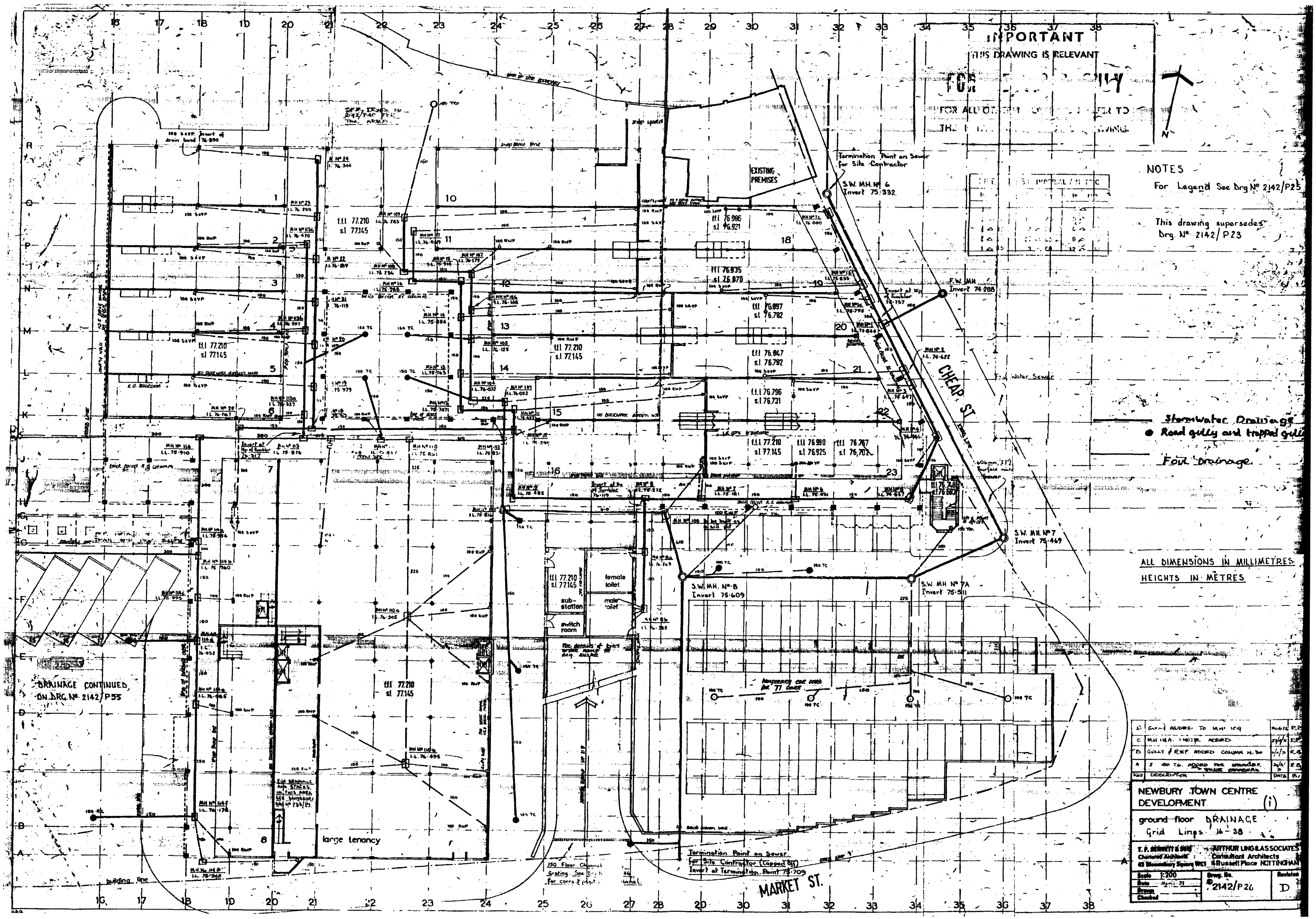
IMPORTANT
THIS DRAWING IS
RELEVANT FOR SERVICES ONLY
FOR ALL OTHER INFORMATION REFER TO THE LATEST ARCHITECTURAL DRAWINGS

SANITATION & RAINWATER

Rev A General Revisions 18.2.81

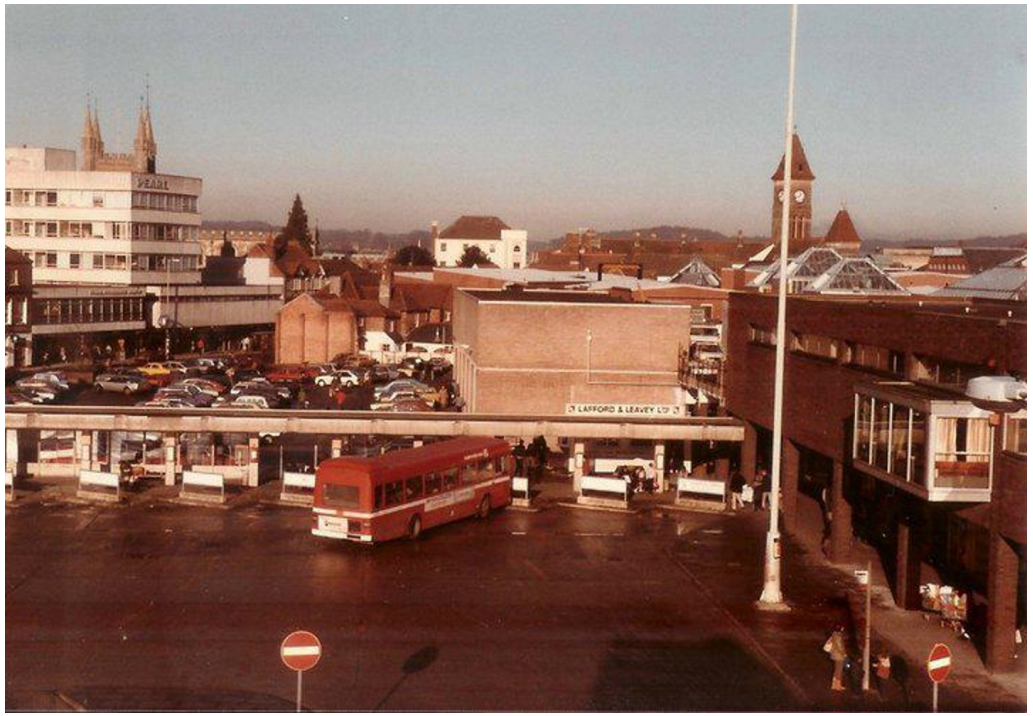
contract title	NEWBURY TOWN CENTRE (Phase 2)
drawing title	Shopping Mall FIRST Floor Plan
scale	1:200
date	8.9.80
drawn	
contract	4479
drawing number	L.0 (52) 11
revision	B

T P Bennett & Son
Chartered Architects
262 High Holborn WC1V 7DU
Telephone 01 405 9277



IMPORTANT
THIS DRAWING IS RELEVANT
NOTES
For Legend See Drg Nº 2142/P25
This drawing supersedes Drg. Nº 2142/P23
Termination Point on Sewer for Site Contractor
EXISTING PREMISES
S.W. MH. Nº 6 Invert 75·332
F.W. MH Invert 74·285
CHEAP ST
Foul Water Sewer
Stormwater Drainage
Road gulley and trapped gully
Foul Drainage
S.W. MH. Nº 7 Invert 75·469
ALL DIMENSIONS IN MILLIMETRES
HEIGHTS IN METRES
S.W. MH. Nº 8 Invert 75·609
S.W. MH Nº 7A Invert 75·511
female toilet
male toilet
sub-station
switch room
DRAINAGE CONTINUED ON DRG. Nº 2142/P55
large tenancy
building line
Termination Point on Sewer for Site Contractor (capped off) Invert at Termination Point 75·709
150 Floor Channel Grating
MARKET ST.
f.f.l 77.210 s.l 77.145
NEWBURY TOWN CENTRE DEVELOPMENT
ground floor DRAINAGE Grid Lines 16 - 38
T. P. BENNETT & SON Chartered Architects
ARTHUR LING & ASSOCIATES Consultant Architects 4 Russell Place NOTTINGHAM
Scale 1:200
Drwg. No. 2142/P26
Revision D

Appendix C
Historic Photos



Robert Bird Group

Title: Pre-Phase 3 Construction Photos			
Project: Kennet Centre			
Job No: 4508	By: ce	Chk: ce	App: ce
Date: 04/10/2019	Status: FOR INFORMATION		
Sketch No:			Rev:

FS 538679

Robert Bird & Partners Limited
Registered in the UK. Company No. 4472743

Level 1
Harling House
47-51 Great Suffolk Street
Southwark, SE1 0BS
United Kingdom

P: +44 (0) 20 7633 2880
F: +44 (0) 20 7928 4477

www.robertbird.com

Robert Bird Group
Member of the Surbana Jurong Group